



US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Safeco Credit Company, Inc.</b>	Address (As shown on registration certificate) <b>Safeco Plaza Seattle, WA 98185</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b>	<b>B. Kind of Agency</b>	<b>C. Certificate No.</b>
<b>Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2, and 3 - CRS No. HEER617E</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

**D.** I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>9-30-95</b>	Signature of Authorized Individual 
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**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>9-30-95</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 	

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

### **INSTALLED THE FOLLOWING EQUIPMENT**

System	Station	Drawing Number
1. 2nd F-F/40 FM Radio	28.53	PHI-11108

Battery and generator are sufficient for electrical load according to AC 43.13-2A, paragraph 27d, Chapter 2.

Wiring was installed in a suitable manner for protection against fuel, oil, water, and other detrimental substances and abrasion damage, AC 43.13-2A, Chapter 2, paragraph 27a(2).

Wires and cables are supported by insulated clamps to prevent chafing according to AC 43.13-1A, Chapter 11, section 7, paragraph 518.

Equipment located where it is sufficiently cooled and would not be a smoke hazard or ignite readily flammable parts of the aircraft, AC 43.13-2A, Chapter 2, paragraph 21a and 27e.

Equipment switches and circuit breakers placarded for radio installation as required by AC 43.13-2A, Chapter 2, paragraph 27c(4).

Adequate means of inspection are provided for racks, related parts, equipment, and adjacent components according to AC 43.13-2A, Chapter 1, paragraph 8.

Antenna installation located and installed similar to AC 43.13-2A, Chapter 3, paragraph 36a, b and 38b. Also followed guidelines of paragraph 42.

Compass corrected as required.

Weight and balance amended as required.

**END**

Additional Sheets Are Attached



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1. Aircraft	Make <i>Bell</i>	Model <i>206L-3</i>
	Serial No. <i>51529</i>	Nationality and Registration Mark <i>US N3116L</i>
2. Owner	Name (As shown on registration certificate) <i>Petroleum Helicopters, Inc</i>	Address (As shown on registration certificate) <i>2001 SE EVANGELINE THRUWAY LA FAYETTE, LA 70508-2156</i>

**3. For FAA Use Only**

**4. Unit Identification**

**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	----- (As described in Item 1 above) -----				
POWERPLANT	<i>Rolls Royce</i> <small>See Item 8</small>	<i>250-C30P</i> <small>See Item 8</small>	<i>CAE 895559</i> <i>CAT 97904</i> <small>See Item 8</small>	X	
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address  STANDARD AERO LIMITED 33 ALLEN DYNE ROAD WINNIPEG, MANITOBA, CANADA R3H 1A1	B. Kind of Agency	C. Certificate No.  AMO 22-58
	U.S. Certificated Mechanic	
	Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date  <i>05 / July / 2002</i>	Signature of Authorized Individual  <i>Gary Rozmus</i> Gary Rozmus
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**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	
Date of APPROVAL  <i>05 / July / 2002</i>		Certificate or Designation No.  AMO 22-58	Signature of Authorized Individual  <i>Gary Rozmus</i> Gary Rozmus	

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**ROLLS-ROYCE 250/C30 2ND STAGE NOZZLE ASSY P/N 23055438 S/N X54373  
THE PRODUCT IDENTIFIED ABOVE HAS BEEN OVERHAULED IN ACCORDANCE WITH ROLLS-ROYCE 250/C30 OVERHAUL MANUAL 14W3 EDITION 2 REVISION 8 DATED 01 NOVEMBER 2001, AMC-OIL 12 REVISION 15 DATED 25 SEPTEMBER 2001 AND THE CURRENT MAINTENANCE RULES OF THE CANADIAN AVIATION REGULATIONS. THE PRODUCT IS APPROVED FOR RETURN TO SERVICE IN COMPLIANCE WITH CAR 571 AND FAR PART 43.17. ALL PERTINENT DETAILS OF THE WORK PERFORMED ARE ON FILE AT THIS ORGANIZATION UNDER WORK ORDER L378290.**

REFERENCE PO: PO0072179

T.T.S.N.: UNK T.T.S.O.: 0.0

SEAL BORE: 1.833" - 1.834"

1ST BLADE PATH: 6.266" - 6.267"

2ND BLADE PATH: 6.693" - 6.694"

FLOW AREA: 6.92

CEB 72-3188 R2 EMBODIED

GARY ROZMUS   
CANADIAN APPROVED INSPECTOR FOR  
STANDARD AERO LTD.  
AMO 22-58

-----END-----

**NOT  
INSTALLED**

Addition Sheets Are Attached





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of Transportation  
Federal Aviation  
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1. Aircraft	Make <i>BELL</i>	Model <i>206L-3</i>
	Serial No. <i>51529</i>	Nationality and Registration Mark <i>US N3116L</i>
2. Owner	Name (As shown on registration certificate) <i>Petroleum Helicopters, Inc</i>	Address (As shown on registration certificate) <i>2001 SE EVANGELINE THRUWAY LA FAYETTE, LA 70508-2156</i>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	----- (As described in Item 1 above) -----				
POWERPLANT	SEE BLOCK#8 <i>Rolls Royce</i>	SEE BLOCK#8 <i>250-C30P</i>	<i>CAE895559</i> SEE BLOCK#8 <i>CAT 97904</i>	X	
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
STANDARD AERO LIMITED 33 ALLEN DYNE ROAD WINNIPEG, MANITOBA, CANADA R3H 1A1	<input type="checkbox"/> U.S. Certificated Mechanic	AMO 22-58
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

MAY/16/2002	Signature of Authorized Individual  <i>GARY ROZMUS</i>
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**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	
MAY/16/2002	Certificate or Designation No. AMO 22-58	Signature of Authorized Individual  <i>GARY ROZMUS</i>		

**NOTICE**

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**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Rolls Royce 250/C30 3<sup>rd</sup> stg. nozzle P/N 23057160 S/N MK56872

The product identified has been overhauled in accordance with Rolls-Royce 250/C30 overhaul manual 14W3 edition 2 revision 8 dated Nov/1/01, Amc-oil 20 revision 8 dated Nov/14/01 and the current maintenance rules of the Canadian Aviation Regulations and Federal Aviation Regulations. The product is approved for return to service in compliance with CAR 571 and FAR PART 43.17.


All pertinent details of the work performed are on file at this organization under Work Order No: L378271

Reference P/O: PO0076988

Flow area 14.91

T.T.S.N. UNK. T.T.S.O. 0.0

THE OWNER AND/OR OPERATOR WHO INSTALLS THIS UNIT MUST COMPLETE BLOCKS ONE AND TWO

GARY ROZMUS   
CANADIAN APPROVED INSPECTOR FOR  
STANDARD AERO LTD.  
AMO 22-58

-----E N D-----

**NOT  
INSTALLED**

Addition Sheets Are Attached



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Federal Aviation Administration

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1. Aircraft	Make	BELL	Model	206L-3
	Serial No.	51529	Nationality and Registration Mark	US N3116L
2. Owner	Name (As shown on registration certificate)		Address (As shown on registration certificate)	
	PETROLEUM HELICOPTERS, INC.		2001 SE EVANGELINE THRUWAY LAFAYETTE, LA 70508-2156	

**3. For FAA Use Only**

**4. Unit Identification**

**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	----- (As described in Item 1 above) -----				
POWERPLANT	See Item 8 Rolls Royce	See Item 8 250-C30P	CAE895559 See Item 8 CAT 97904	X	
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address  STANDARD AERO LIMITED 33 ALLEN DYNE ROAD WINNIPEG, MANITOBA, CANADA R3H 1A1	B. Kind of Agency	C. Certificate No.  AMO 22-58
	<input type="checkbox"/> U.S. Certificated Mechanic	
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Date	Signature of Authorized Individual
08 / July / 2002	Gary Rozmus

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of APPROVAL	Certificate or Designation No.	Signature of Authorized Individual		
08 / July / 2002	AMO 22-58	Gary Rozmus		

**NOTICE**

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**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**ROLLS-ROYCE 250/C30 4TH STAGE NOZZLE ASSY. P/N 23032264 S/N X59788  
THE PRODUCT IDENTIFIED ABOVE HAS BEEN OVERHAULED IN ACCORDANCE WITH ROLLS-ROYCE 250/C30  
OVERHAUL MANUAL 14W3 EDITION 2 REVISION 8 DATED 01 NOVEMBER 2001, AMC-OIL 19 REVISION 9  
DATED 05 FEBRUARY 1999 AND THE CURRENT MAINTENANCE RULES OF THE CANADIAN AVIATION  
REGULATIONS. THE PRODUCT IS APPROVED FOR RETURN TO SERVICE IN COMPLIANCE WITH CAR 571  
AND FAR PART 43.17. ALL PERTINENT DETAILS OF THE WORK PERFORMED ARE ON FILE AT THIS  
ORGANIZATION UNDER WORK ORDER L378347.**

**REFERENCE PO: PO0078837**

**T.T.S.N.: UNK T.T.S.O.: 0.0**

**SEAL BORE: 2.627" - 2.628"**

**3RD BLADE PATH: 8.078" - 8.079"**

**4TH BLADE PATH: 9.000" - 9.001"**

**FLOW AREA: 22.70**

**GARY ROZMUS**   
**CANADIAN APPROVED INSPECTOR FOR**  
**STANDARD AERO LTD.**  
**AMO 22-58**

-----**END**-----

**NOT  
INSTALLED**

Addition Sheets Are Attached

PHI, INC. LAFAYETTE, LA CRS HEER617E OPTIONAL EQUIPMENT LIST  
 BELL 206L SERIES, N233PH, S/N 51529, DATED 7-12-2012

ITME/PART NUMBER	WEIGHT LBS.	ARM IN.	MOMENT LBS/IN.	SUPP	FORM 337
SKYNAV #497 TRAFFIC	132.25	144.02	1908.30	_X_	_X_
A) TRANSCEIVER	10.95	156.00	1708.20		
B) ANTENNA	2.30	87.00	200.10		
KCS 55/55A PICTORIAL NAV SYSTEM	8.90	99.82	888.40		
A) DIRECTIONAL GYRO	4.30	153.00	657.90		
B) HORIZONTAL SITUATION INDICATOR	4.00	38.20	152.80		
C) SLAVE	0.30	39.00	11.70		
D) FLUX VALVE	0.30	220.00	66.00		
GARMIN GNS 420 SYSTEM (STC SR02232LA)	6.50	46.69	303.48	_X_	_X_
A) CONTROL DISPLAY UNIT	4.50	41.00	184.50		
B) MOUNTING FRAME	0.80	41.00	32.80		
C) GPS ANTENNA	0.40	85.25	34.10		
D) #1 VHF ANTENNA	0.80	65.10	52.08		
KY196 #2	3.70	68.65	254.00		_X_
A) TRANSCEIVER	3.20	41.00	134.00		
B) ANTENNA	0.50	240.00	120.00		
CP-2 OUTERLINK SAT COM SYSTEM (STC SR00120BO)	8.80	176.33	1551.70	_X_	_X_
A) OUTERLINK CP-2	6.50	185.00	1202.50		
B) OUTERLINK ANTENNA	0.30	396.00	118.80		
C) OUTERLINK ANTENNA	0.30	210.00	63.00		
D) OUTERLINK FILTER	0.70	182.00	127.40		
E) OUTERLINK CONTROL	1.00	40.00	40.00		
GARMIN GDL 69	3.90	68.02	210.19		_X_
A) RECEIVER	1.72	76.50	131.58		
B) RACK	0.97	76.50	74.21		
C) ANTENNA	0.40	11.00	4.40		
TFM550 FM SYSTEM	6.54	58.14	380.23		
A) TRANSMITTER	5.04	38.20	192.53		
B) ANTENNA UHF FM	0.50	32.00	16.00		
C) ANTENNA VHF FM	0.50	181.70	90.85		
D) ANTENNA LOW BAND	0.50	161.70	80.85		
KT76A TRANSPONDER	3.60	40.08	144.30		_X_
A) TRANSCEIVER	3.10	43.00	133.30		
B) ANTENNA (KA-60)	0.50	22.00	11.00		
BLIND ENCODER (MODEL SS120-30)	0.60	22.00	14.30		
NORTHERN AIRBORNE ICS	2.30	44.00	74.80		
A) AUDIO CONTROL (AMS-44)	2.30	44.00	74.80		
B) 5 STATION (WIRING ONLY)	NO WEIGHT CHANGE				

PHI, INC. LAFAYETTE, LA CRS HEER617E OPTIONAL EQUIPMENT LIST  
 BELL 206L SERIES, N233PH, S/N 51529, DATED 7-12-2012

ITME/PART NUMBER	WEIGHT	ARM	MOMENT	SUPP	FORM
	LBS.	IN.	LBS/IN.		337
AVIONICS MASTER	0.50	76.00	36.50	_____	<u>  X  </u>
MP3 PLAYER	WEIGHT CHANGE NEGLIGIBLE			_____	_____
RA 4500 RADAR ALTIMETER	3.73	157.78	588.82	_____	<u>  X  </u>
A) RA4500 RCV/XMTR	1.90	202.00	383.80		
B) RAD-40 INDICATOR	0.60	38.00	22.80		
C) AUDIO MIXING AMP	0.35	58.50	20.47		
D) AURAL WARNING GEN	0.38	58.50	22.23		
E) ANTENNA	0.25	268.30	67.07		
F) ANTENNA	0.25	289.80	72.45		
HEADSETS (1 EACH)	1.30	65.00	84.50	_____	_____
REAR CABIN HEADSETS (1 EACH)	1.30	129.00	167.70	_____	_____
REAR CABIN CURLEY CORDS (1 EACH)	0.50	129.00	64.50	_____	_____
NAT CABIN PA SYSTEM	3.40	86.70	294.80	_____	_____
A) NAT AA21-400 AMPLIFIER	1.40	42.00	58.80		
B) CABIN SPEAKER (REAR 2 EACH)	2.00	118.00	236.00		
DAVTRON M450A VOLTMETER	0.25	39.00	9.75	_____	_____
ELECTRIC ARTIFICIAL HORIZON	3.00	40.00	120.00	_____	_____
DAVTRON M800 DIGITAL CLOCK	0.60	40.00	24.00	_____	_____
INTELLISTART SYSTEM (STC SR00113BO)	7.34	134.73	983.54	<u>  X  </u>	<u>  X  </u>
A) PROCESSOR ASSEMBLY	1.10	185.00	203.50		
B) ENGINE HARNESS KIT	3.40	92.00	312.80		
C) ENGINE FUEL TRIM SOLENOID VALVE KIT	1.40	192.00	268.80		
D) ENGINE TORQUE KIT	0.68	155.00	105.40		
E) AIRCRAFT OUTSIDE AIR TEMPERATURE PROBE KIT	0.36	194.00	69.84		
F) COCKPIT FAULT LAMP ASSEMBLY KIT	0.20	43.00	8.60		
G) ADDITIONAL INSTALLATION MATERIAL	0.20	73.00	14.60		
STARTER SEQUENCE	0.50	127.00	63.56	_____	<u>  X  </u>
A) MICRO SWITCH	0.10	73.00	7.30		
B) BRACKET	0.10	73.00	7.30		
C) CONTROL BOX	0.30	163.20	48.96		
RECONFIGURED INSTRUMENT PANEL (STC SR09594RC)	NEGLIGIBLE WEIGHT CHANGE			<u>  X  </u>	<u>  X  </u>
AVIONICS/TIEDOWN, SUPPORT RACK	4.50	25.00	112.50	_____	<u>  X  </u>
BAGGAGE DOOR & TIEDOWN WARNING	USE ACTUAL WEIGHT			_____	_____
WHELEN A413A HAD-DF-28 STROBE LIGHT POWER SUPPLY	3.00	196.90	590.70	_____	_____

PHI, INC. LAFAYETTE, LA CRS HEER617E OPTIONAL EQUIPMENT LIST  
 BELL 206L SERIES, N233PH, S/N 51529, DATED 7-12-2012

ITME/PART NUMBER	WEIGHT LBS.	ARM IN.	MOMENT LBS/IN.	SUPP	FORM 337
FACET EXTERNAL OIL FILTER	4.00	188.00	785.00	___	_X_
PARAVION HEATER (STC SH2728NM)	11.39	97.00	1105.00	_X_	_X_
CONCORDE LEAD ACID BATTERY (STC SR09069RC)	40.00	15.80	632.00	_X_	_X_
STEP HANDLES (STC SH1366SO)	2.00	107.13	214.26	___	_X_
FOLDING STEPS (STC SH2243SO) L/H ONLY	2.00	145.18	290.36	___	_X_
ALUMINUM ENTRANCE STEPS (STC SH1322SO)	USE ACTUAL WEIGHT			___	_X_
PNEUMATIC DOOR ACTUATORS (STC SH1990SO)	5.90	87.24	514.70	___	_X_
A) FRONT DOOR ACTUATORS (2 EACH)	2.60	42.00	190.20		
B) PASSENGER DOOR ACTUATORS (2 EACH)	2.00	104.00	208.00		
C) BAGGAGE DOOR ACTUATOR (1 EACH)	1.30	155.00	201.50		
WINDSHIELDS (STC SH1664SO)	NEGLIGIBLE WEIGHT CHANGE			___	_X_
SKYLIGHTS (STC SH1664SO)	NEGLIGIBLE WEIGHT CHANGE			___	_X_
POP-OUT WINDOWS (STC SH7608SW)	NO WEIGHT CHANGE			___	_X_
FLOOR PROTECTORS (RHINO LINER)	7.00	44.00	308.00	___	___
A) CREWL/H & R/H	7.00	44.00	308.00	___	___
POP-OUT WINDOWS (STC SH5773SW)	NO WEIGHT CHANGE			___	_X_
POP-OUT CHIN BUBBLE WINDOWS (STC SH5850SW)	NO WEIGHT CHANGE			___	_X_
PASSENGER DOOR RUBBER MOUNTED WEDGE WINDOW (STC SH5685SW)	NO WEIGHT CHANGE			___	_X_
PULSELITE CONTROL (STC SH3319NM)	1.00	21.00	21.00	_X_	_X_
AIR DEFELCTORS (STC SR01582AT)	1.36	85.00	115.60	___	_X_
A) CREW (2 EACH)	0.68	60.00	40.80		
B) PASSENGER (2 EACH)	0.68	110.00	74.80		
INSTALLED LORD TAIL ROTOR BEARING KIT (STC SR09248RC)	2.10	405.20	850.92	___	_X_
APICAL FLOAT/RAFT SYSTEM (STC SR01535LA) WITH FLARES INSTALLED	USE ACTUAL WEIGHT			_X_	_X_
HIGH SKID GEAR SI206-2049 (206-706-064-105)	113.40	104.50	11850.00	_X_	___
DUAL ROTOR BRAKE SI206-2044 (206-076-502-101)	13.90	130.60	1815.00	___	___
CARGO HOOK PROVISIONS SI206-2012 (206-706-341-9)	3.20	97.60	311.00	_X_	___

PHI, INC. LAFAYETTE, LA CRS HEER617E OPTIONAL EQUIPMENT LIST  
 BELL 206L SERIES, N233PH, S/N 51529, DATED 7-12-2012

ITME/PART NUMBER	WEIGHT LBS.	ARM IN.	MOMENT LBS/IN.	SUPP	FORM 337
ARTEX 406-2 HM ELT SYSTEM PROVISIONS	USE ACTUAL WEIGHT			_____	_____
DUKANE PINGER	1.00	225.00	225.00	_____	_X_
LIFE VEST (1 EACH)	1.50	65.00	97.50	_____	_____
LIFE VEST (2 EACH)	3.00	91.00	273.00	_____	_____
LIFE VEST (3 EACH)	4.50	129.00	580.50	_____	_____
FIRE EXTINGUISHER ON FWD SIDE VERTICAL TUNNEL	2.40	82.00	194.40	_____	_____
PASSENGER FOOT REST	2.00	38.50	77.00	_____	_____
TUB RHINO LINER & LH/RH NOMEX PROTECTORS	12.30	110.00	1353.00	_____	_____
A/C TIE DOWN KIT	11.00	25.00	275.00	_____	_____
MAIN ROTOR TIE DOWN KIT	8.00	178.00	1424.00	_____	_____
SOUND SUPPRESSORS	1.25	110.00	137.50	_____	_____
BAGGAGE COMPARTMENT FLOOR (ROYALITE)	6.00	168.00	1008.00	_____	_____
REAR VIEW MIRROR L/H	0.30	28.00	8.40	_____	_____
REAR VIEW MIRROR R/H	0.30	28.00	8.40	_____	_____
BAGGAGE COMPARTMENT CARGO NET	3.50	188.00	658.00	_____	_____
BHT-206L-SI-2052, INCREASED GROSS WEIGHT KIT	USE ACTUAL WEIGHT			_____	_X_

END

Thomas Lane   
 \_\_\_\_\_  
 MECHANIC NAME (PRINTED)

8101  
 \_\_\_\_\_  
 EMPLOYEE NUMBER



BELL MODEL 206LS4 (206L-3 with BHT-206-SI-2052)  
WEIGHT AND BALANCE

[Click here to clear sheet](#)



**Step 1.**

Make: Bell  
 Aircraft "N" Number: N233PH Model: 206L-3+ Aircraft S/N: 51529  
 Equipment installed when weighed:  
 Type of Landing Gear: High Skid Gear - Apical Floats  
 Name of Scales: Road Runner Scale S/N: TE-1268  
 Scale Calibration Date: 05/2012  
 Location of Weighing: PHI INC. Lafayette La  
 Weighed with unusable fuel, all oils serviced full and No Ballast? yes Make a note here of weight & arm if weighed with ballast. Do not enter in the white blocks (Step 3 below)

**Step 2.**

**Scale Weight Area**

Enter all scale readings here.

Note: Enter scale corrections in the 2nd column (S/C) if required.

SCALE READINGS (3 EACH JACKPOINTS)	SCALE	S/C	NET
A. (FS 55.16, BL -16.82) Enter the L/H Fwd (Red) jack reading here ----->	699.0		699.0
B. (FS 55.16, BL +16.82) Enter the R/H Fwd (Green) jack reading here ----->	731.0		731.0
C. (FS 204.92, BL 0) Enter the Aft (Yellow) jack reading here ----->	1413.0		1413.0
	<b>TOTAL</b>		<b>2843.0</b>

**LONGITUDINAL C.G. AS WEIGHED:**

$$FS\ 55.16\ in\ \times\ (1430.0\ )\ LB\ +\ FS\ 204.92\ in\ \times\ (1413.0\ )\ LB\ =\ 368430.76\ =\ 129.59$$

Total Weight (lbs) 2843.0

**LATERAL C.G. AS WEIGHED:**

For Lateral C.G.: Left (-), Right (+)

$$FS\ +16.82\ in\ \times\ (731.0\ )\ LB\ +\ FS\ -16.82\ in\ \times\ (699.0\ )\ LB\ =\ 538.24\ =\ 0.19$$

Total Weight (lbs) 2843.0

D. Note: Review C.G. Chart.

**Step 3.**

**Ballast Area**

When the actual C.G. (Arm) is not within the forward and aft limits on the C.G. Chart, determine the C.G. (Arm) required and enter the desired C.G. in the Blue "Desired C.G." block in this section. Note the weight in the "Lndg Light, Battery, Console's or Midboom" blocks and enter the appropriate ballast in the corresponding BLUE box to the right. **Recheck the C.G. Page** and verify that you are within the forward and aft limits. If not correct, make appropriate correction.

	As Weighed	Weighted Arm	Current Weight	Current Arm	Desired C.G.
Empty Weight =	2843.0	129.59	2869.0	128.54	

	DESCRIPTION	WEIGHT	LONGITUDINAL		LATERAL		
	Units of measure	Weight, Lbs	ARM, IN	MOMENT, in-lb	ARM, in	MOMENT, in-lb	
Landing Light Ballast	0.0						
	Empty Weight	2843.0	129.59	368431	0.19	538	
	Add unusable fuel		94.00	0	0.00	0	
Battery Ballast	0.0		13.00	338	-2.50	-65	Max 28 Lbs.
	Lndg Light Ballast	26.0	16.40	0	4.60	0	Max 22 Lbs.
	Battery Ballast		29.80	0	1.00	0	See Note 5
Fwd Console Ballast	0.0		35.80	0	1.00	0	See Note 6
	Aft Console Ballast			0	0.00	0	See Note 7 & 8
	Midboom Ballast			0		0	
	Total	2869.0	128.54	368769	0.16	473	

Aft Console Ballast	0.0
Midboom Ballast	0.0

**Notes**

Note 1: To maintain a standard. It is better to remove weight than add weight if it is possible.

Note 2: Never install Fwd Fuselage and Mid tailboom ballast installed at the same time.

Note 3: Unusable fuel: Weight = 7.6 pounds, Arm = 94.0 inch.

Note 4: If Lateral arm exceeds 1 inch, contact Lafayette QA.

Note 5: Aircraft S/N 51390 and Subsequent, total weight of ballast not to exceed 50 pounds.

Note 6: Aircraft S/N 51001 thru 51389, total weight of ballast not to exceed 30 pounds

Note 7: Total weight of ballast not to exceed 20 pounds with a maximum height of 0.75 inch.

Note 8: Possible arms for midboom ballast are 279.15, 282.70, 286.25, 287.75, 294.85 & 291.30 -- Please refer to Maintenance Manual for details and possible combinations. The recommended ballast on this sheet is figured from station 291.30, you must enter the longitudinal arm you use, here. Slight ballast (weight) adjustments may be required if you use an arm less than 291.30. Lateral midboom ballast arm is always 0.

July 13, 2012  
Date Aircraft Weighed

Thomas Lage *Thomas Lage* CRS HEER617E  
Print Name Signature Certificate Number  
Of Person doing the weighing.

July 13, 2012  
Date Weight & Balance Checked

Kevin Latiolais *Kevin Latiolais* CRS HEER617E  
Print Name Signature Certificate Number  
Of Person checking the math and CG.





**Model:** 206L-3  
**AC Reg No:** N233BH  
**AC SrlNo:** 51529

**PHI, INC**  
**Aircraft Status Report**

AC Base: 99L-NEW IBERIA, LA

AC TT: 21,397:37  
 AC TO: 99,714

Mfr. Date: 11/12/1991  
 Last Flt Date: 03/31/2018

Regulatory/NonRegulatory Flag: ALL

Pos	Work Unit	Part #	Part Desc.	Part Serial	Tol +/-	Service Life Usage	Date	T.T T.C	TS-IN/OH CS-IN/OH	Rem Units	Due At A/C Hr.	Date	Tol +/-
	<u>1200H/12M-A/F</u>					<u>1200 FH</u>	12 ME			886.40	22,284:01	05/31/18	
	150H-A/F					150 FH				60.00	21,457:37		
	300H-A/F					300 FH				177.42	21,575:01		
	600H-A/F					600 FH				335.92	21,733:31		
	<u>ASB 206L-89-61 IN</u>						2 YME					07/31/18	
	MULTI-600H-ENG-OIL					600 FH				349.90	21,747:31		
	AIRCRAFT-REG-EXPIRATION						3 YME					09/30/22	
	400H-IGW-T/R-G/BOX-OIL					400 FH				98.48	21,496:05		
	400H-IGW-TRANS-OIL					400 FH				219.05	21,616:40		
	<u>206L-3 12M ELT</u>						12 ME					07/31/18	
	<u>206L-3 18M APICAL FLOAT</u>						18 ME					07/31/18	
	<u>206L-3 24M ALT REPSYS</u>						24 ME					07/31/18	
	<u>206L-3 300H/6M FACET FILTER</u>					<u>300 FH</u>	6 MO			262.33	21,659:56	06/02/18	
	<u>206L-3 3Y APICAL FLOAT</u>						3 YME					07/31/18	
	206L-3 50H A/F					50 FH				23.47	21,421:04		
	<u>206L-3 6M EMERGENCY EQUIP</u>						6 MO					05/28/18	
	<u>206L-3 6M FIRE EXT</u>						6 MO					05/27/18	
	<u>PRJ-181226-PHI INC DECAL</u>						36 D					01/31/19	
	AD-2018-25-17					21487 FH				90.00	21,487:37		
	<u>PRJ-190319-PHID-2071 DECAL</u>						7 D					03/26/19	
	<u>ASB-206L-19-182</u>					<u>21497 FH</u>	90 D			100.00	21,497:37	07/02/19	
	AD-2019-08-10					21497 FH				100.00	21,497:37		
	ASB-206L-19-183					21417 FH				20.00	21,417:37		
	ASB-206L-19-181					21697 FH	384 D			300.00	21,697:37	09/15/20	
	<u>PRJ-190909-BLUE TORCH</u>						7 D					09/16/19	
212101030-L		206-073-913-001	Blower, Assembly (01932)	9902	-				5,214.52				
212101030-R		206-073-913-001	Blower, Assembly (01932)	09-10-2011	-				5,214.52				
<u>230501010</u>	<u>CHG-PN-TFM-550 28V TO 28V NVG</u>	<u>991095-1(28V)</u>	<u>Transceiver, Tfm550</u>	<u>JD2051</u>	<u>-OTH</u>	<u>3648 D</u>						07/21/18	
231201010-#1		064-1054-30	Transceiver, Ky196a		-								



Model: 206L-3  
 AC Reg No: N233BH  
 AC SrlNo: 51529

**PHI, INC**  
**Aircraft Status Report**

AC Base: 99L-NEW IBERIA, LA

AC TT: 21,397:37  
 AC TO: 99,714

Mfr. Date: 11/12/1991  
 Last Flt Date: 03/31/2018

Regulatory/NonRegulatory Flag: ALL

Pos	Work Unit	Part #	Part Desc.	Part Serial	Tol +/-	Service Life Usage Date	T.T T.C	TS-IN/OH CS-IN/OH	Rem Units	Due At A/C Hr. Date	Tol +/-
231201010-#2		064-1054-30	Transceiver, Ky196a	4080	-		100,276.85				
231201020		011-01058-00	Transceiver, Gnc-420w	23220589	-		7,983.87				
<b>243201010</b>	<b>LEAD ACID BATTERY INSPECTION</b>	<b>RG-222</b>	<b>(Stc) Battery, Lead Acid (01932)</b>	<b>40868493</b>	<b>600 FH-INS</b>	<b>12 MO</b>	<b>37.67</b>	<b>37.67</b>	<b>562.33</b>	<b>21,959:56</b>	<b>12/18/18</b>
243501010-#1	STARTER-GEN-INSP	23081-018	Starter Generator (00363)	1779	500 FH-INS		21,033.19	289.93	210.07	21,607:40	
243501010-#1	STARTER-GEN-OH	23081-018	Starter Generator (00363)	1779	1000 FH-OVH		21,033.19	615.35	384.65	21,782:15	
256201010		453-5001-366	Elt, C406-2hm		-						
256201020	D/S/P UNIT IN/BAT 6 YR RT	DK120	Pinger, Sonar	SC41306	-INS	6 YME				10/31/20	
<b>256201020</b>	<b>ULB TEST AND INSP</b>	<b>DK120</b>	<b>Pinger, Sonar</b>	<b>SC41306</b>	<b>-INS</b>	<b>6 MO</b>				<b>04/20/18</b>	
281001010		1743640-01	(Step) Filter And Housing Assy, Ext. Fuel	4603	-		578.32				
282201020		1C36-1	Pump, Boost	3AG33	-		105,214.52				
282203010-L		2C27-1	Cartridge, Fuel Pump	12AG441	-		105,214.52				
282203010-R		2C27-1	Cartridge, Fuel Pump	2F0028	-		105,214.52				
284401010		206-075-676-119	Indicator, Fuel Press Loadmeter (01932)	597	-		5,214.52				
291201010		206-076-034-103	Filter Assy, Hyd Pressure (01932)	3140	-		5,214.52				
291301020		206-076-035-103	Filter Assy, Hyd Return (See Remarks)	1749	-		5,214.52				
311201010		206-075-681-123	Indicator, Dual Tachometer	01797	-		5,214.52				
311201020		206-075-682-107	Indicator, Gas Producer, Rod (00363)	01083	-		4,343.73				
311401020		206-075-739-113	Indicator, Torquemeter	B377	-		102,511.93				
313601010		DPU-A-010-1-K	(Step) Processor, Intellistart	0369	-		101,301.19				
321201010		APICAL-206	Landing Gear Assembly	028	-		108,377.44				
321201040		647.5002	(Step) Reservoir Assy, 206L-L4 Lift Raft	0018	-		3,924.56				
<b>321202010</b>	<b>RAFT-VALVE-206-3-YR-OH</b>	<b>645.5902-090-216-01</b>	<b>(Step) Valve, (Life Raft)</b>	<b>10018</b>	<b>-OVH</b>	<b>3 YME</b>				<b>07/31/18</b>	
<b>321202020</b>	<b>CYLINDER 1104 15 YR RT</b>	<b>601.1104</b>	<b>(Step) Cylinder, Life Raft</b>	<b>TS129</b>	<b>-RTR</b>	<b>15 YME</b>				<b>03/31/19</b>	
<b>321202020</b>	<b>CYLINDER 1104 HYD TEST</b>	<b>601.1104</b>	<b>(Step) Cylinder, Life Raft</b>	<b>TS129</b>	<b>-INS</b>	<b>5 YME</b>				<b>07/31/19</b>	
321202020		647.5202	(Step) Reservoir Assy, 206L-L4 Float	11045	-		103,683.10				
<b>321202010</b>	<b>FLOAT-VALVE-206-3-YR-OH</b>	<b>645.5905-135-216-08</b>	<b>(Step) Valve, (Float)</b>	<b>10108</b>	<b>-OVH</b>	<b>3 YME</b>				<b>07/31/18</b>	
<b>321202020</b>	<b>CYLINDER 0749 15 YR RT</b>	<b>600.0749</b>	<b>(Step) Cylinder, Float</b>	<b>KN276</b>	<b>-RTR</b>	<b>15 YME</b>				<b>03/31/19</b>	
<b>321202020</b>	<b>CYLINDER 0749 HYD TEST</b>	<b>600.0749</b>	<b>(Step) Cylinder, Float</b>	<b>KN276</b>	<b>-INS</b>	<b>5 YME</b>				<b>04/30/18</b>	



U.S. Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR 43 Sec.43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. Sec. 44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. Sec. 46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N 233PH</u>	Serial No. <u>51529</u>	
	Make <u>Bell</u>	Model <u>206</u>	Series <u>L-31</u>
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHB Inc</u>	Address (As shown on registration certificate) <u>2001 SE Evangeline Thruway</u>	
		City <u>Katoyette</u>	State <u>LA</u>
		Zip <u>70508-2156</u>	Country <u>USA</u>

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	~~~~~	(As described in Item 1 above)	~~~~~
<input checked="" type="checkbox"/>	<input type="checkbox"/>	POWERPLANT	Rolls Royce	250 C-30P	CAE-895683 CAG-90183
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <u>H-S Tool &amp; Parts Inc.</u>	Address <u>#140-2560 Simpson Road</u>	<input type="checkbox"/> U.S. Certified Mechanic	Manufacturer
City <u>Richmond</u> State <u>BC</u>		<input checked="" type="checkbox"/> Foreign Certified Mechanic	C. Certificate No.
Zip <u>V6X 2P9</u> Country <u>Canada</u>		<input type="checkbox"/> Certified Repair Station	<b>39-87</b>
		<input type="checkbox"/> Certified Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>C. Trsek</u> <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">H-S T</span>	23/Nov/2012
--	--	-------------

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	<input checked="" type="checkbox"/>	Persons Approved by Canadian Department of Transport
	FAA Designee	Repair Station	Inspection Authorization		Other (Specify)
Certificate or Designation No. <b>39-87</b>		Signature/Date of Authorized Individual <u>C. Trsek</u> <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">H-S T</span>		23/Nov/2012	



## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N 233 PH

Nationality and Registration Mark

7-22-13

Date

Description: Rolls Royce 250 Gearbox Cover  
P/N 23064606, S/N HL42989

1. Flame Sprayed 3 And 4 Bearing Cage Housing And Outer Flange IAW 14W3 72-60-00 Table 301.
2. Installed (Qty 9) Splitline Bushings IAW 14W3 72-00-00.
3. Locally NDT (FPI) IAW 14W3 72-00-00.
4. Final Inspection.

All Records of Work Performed Are On File Under Work Order 70541.

No Further Statements to Follow.

Additional Sheets Are Attached



U.S. Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR 43 Sec.43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. Sec. 44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. Sec. 46301(a))

1. Aircraft	Nationality and Registration Mark <u>N 233PH</u>	Serial No. <u>51529</u>	
	Make <u>Bell</u>	Model <u>206</u>	Series <u>L-3+</u>
2. Owner	Name (As shown on registration certificate) <u>PHI Inc</u>	Address (As shown on registration certificate) <u>2001SE Evangeline thruway</u>	
		City <u>Lafayette</u>	State <u>LA</u>
		Zip <u>70504-2156</u>	Country <u>USA</u>

3. For FAA Use Only

Blank area for FAA use only.

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	~~~~~	(As described in Item 1 above)	~~~~~
<input checked="" type="checkbox"/>	<input type="checkbox"/>	POWERPLANT	Rolls Royce	250 C-30P	CAE-845683 CAG-90183
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <u>H-S Tool &amp; Parts Inc.</u>	Address <u>#140-2560 Simpson Road</u> City <u>Richmond</u> State <u>BC</u> Zip <u>V6X 2P9</u> Country <u>Canada</u>	<input type="checkbox"/> U.S. Certified Mechanic	<input type="checkbox"/> Manufacturer
		<input checked="" type="checkbox"/> Foreign Certified Mechanic	C. Certificate No.
		<input type="checkbox"/> Certified Repair Station	<b>39-87</b>
		<input type="checkbox"/> Certified Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>J. Gustafson</u> <u>31/Oct/2012</u>
--	---

7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector	Manufacturer	Maintenance Organization	<input checked="" type="checkbox"/> Persons Approved by Canadian Department of Transport
	FAA Designee	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>39-87</b>	Signature/Date of Authorized Individual <u>J. Gustafson</u> <u>31/Oct/2012</u>
--	---

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N233PH

7-22-13

Nationality and Registration Mark

Date

Description: Rolls Royce 250 Gearbox Housing  
P/N 23064603, S/N PC19875

1. Inspected As Per Customers Request IAW 14W3 72-60-00.
2. Repaired Starter Generator Garlock Sealbore IAW 14W3 72-00-00 And LDA1233-GDS-1003.
3. Final Inspection.

All Records of Work Performed Are On File Under Work Order 70033.

No Further Statements to Follow.

Additional Sheets Are Attached





US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
---	---	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC. 1991</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
-----------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>19 DEC. 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
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## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. DESCRIPTION OF WORK ACCOMPLISHED** *(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Installed aft cabin floor cargo restraint kit. See PHI drawing #206L-523.

Restraints are flame resistant and meet AC43.13-2A Para. 242C and FAR 25.853 (b2).

Structural attachment and fittings are of the approved type. AC43.13-2A Para. 244.

Load tested according to AC43.13-2A Para. 245 and 246B.

Restraints placard for 100 lbs. max., according 43.13-2A Para. 247A.

Weight and balance computed.

ADDITIONAL SHEETS ARE ATTACHED

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

US Department  
of Transportation  
Federal Aviation  
Administration

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>..... (As described in item 1 above) .....</i>			X	
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
<b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>HEER617E Airframe, PowerPlant &amp; Accessories Ltd. Radio Class 1,2,3</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-6-2001</b>	Signature of Authorized Individual <i>Dean Hurd</i>
--------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-6-2001</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>Dean Hurd</i>
---	---	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

# Structural Repair

## Aft Lower Fairing Repair

Part Nomenclature:	<u>Aft Lower Fairing</u>	Part Number:	<u>206-033-310-ALL</u>
Repair Number:	<u>H6057-206L-091490</u>	Part Serial Number	<u>None</u>
	<u>Rev. A dated 5-27-91</u>		

Cut out damaged area on Aft Lower Fairing at approximate station 155.00 and installed 17 inch repair in accordance with repair # H6057-206L-091490.

Structural repairs made according to the manufacturer's helicopters structural repair manual, except the size of some repairs exceeded the limits as specified in the manual.

Approval received for these repairs according to data approved on FAA Form 8110-3			
By DER	<u>Eugene L. Turner</u>	Designation Number	<u>SW-566</u>
Dated	<u>6-04-1991</u>	Serial Number	<u>91-023</u>

Weight and Balance amended.



Additional Sheets Are Attached

US Department of Transportation  
Federal Aviation Administration

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

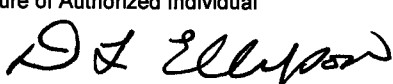
5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	B. Kind of Agency U.S. Certificated Mechanic Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station Manufacturer	C. Certificate No. <b>HEER617E Airframe, PowerPlant &amp; Accessories Ltd. Radio Class 1,2,3</b>
--	---	---

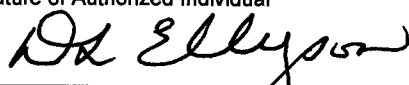
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>12-06-01</b>	Signature of Authorized Individual 
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7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>12-06-01</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 
--	---	--

## NOTICE

Weight and balance or operating limitations changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed Aluminum Floats Step manufactured by Aeronautical Accessories according to STC SH1322SO.

Weight and balance amended.

END

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell Helicopter</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>C I T Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas FL 21 New York, Ny 10036-8701</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b>	<b>B. Kind of Agency</b>	<b>C. Certificate No.</b>
<b>Petroleum Helicopters, Inc.</b> <b>P.O. Box 90808</b> <b>Lafayette, LA 70509</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>Airframe, Powerplant, and            Accessories Limited - Radio Class            1, 2, and 3 - CRS No. HEER617E</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>22-Oct-98</b>	Signature of Authorized Individual 
--------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>22-Oct-98</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 
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**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**ANTENNA DOUBLER**

**LARSEN NMO LOW BAND INSTALLATION**

**Aircraft Models 206, L-1, L-3, and L-4**

**T.T. 6266:40**

**Tail Boom P/N 206-033-004-177 S/N 52037**

Installed Antenna doubler on bottom inside surface of tailboom skin fuselage station 240 inches aft of datum. Doubler installed according to Petroleum Helicopters, Inc. drawing PHI-206-60067, latest revision.

Approval for this modification received on FAA Form 8110-3 in support of repair station return to service. Data approved by DER Michelle M. Corning, Designation number SW-777, dated July 2, 1996.

**END**

Additional Sheets Are Attached





US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
**For FAA Use Only**  
Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas Fl 21 New York, NY 10036-8701</b>

**3. For FAA Use Only**

**4. Unit Identification**

Unit	Make	Model	Serial No.	5. Type	
				Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b>	<b>B. Kind of Agency</b>	<b>C. Certificate No.</b>
<b>Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2 and 3 - CRS No. Heer617E</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

**D.** I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>4-22-99</b>	Signature of Authorized Individual 
------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

<b>BY</b>	FAA Fit Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>4-22-99</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 	

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### INSTALLED THE FOLLOWING EQUIPMENT

<u>Model</u>	<u>Station</u>	<u>Drawing Number</u>	<u>Circuit Breaker (amp.)</u>
1. Artex ELT 110-4 Emergency Locator Transmitter	19.00	PHI-206-12235	½ Fuse

Followed guidelines of FAA Approved Petroleum Helicopters, Inc Electrical Specification & Standard Practices Manual.

Installation made to withstand loads as per FAA advisory circular AC 43.13-2A, Chapter 1, paragraph 2.

Electrical load of the revised system will not exceed 80% of the total generated output as checked by a Preflight Ground run Check made on date 4-23-97 by RLD AMU Certificate number 1840294043As required by AC 43.13-1B, Chapter 11, section 3, paragraph's 30, 36 and 37.

Wiring was installed in a suitable manner for protection against fuel, oil, water, and other detrimental substances and abrasion damage, AC 43.13-2A, Chapter 2, paragraph 27a(2).

Wires and cables are supported by insulated clamps to prevent chafing according to AC 43.13-1B, Chapter 11, section 11, paragraph 146.

Equipment located where it is sufficiently cooled and would not be a smoke hazard or ignite readily flammable parts of the aircraft, AC 43.13-2A, Chapter 2, paragraph 21a and 27e.

Equipment switches and circuit breakers placarded for radio installation as required by AC 43.13-2A, Chapter 2, paragraph 27c(4).

Adequate means of inspection are provided for racks, related parts, equipment, and adjacent components according to AC 43.13-2A, Chapter 1, paragraph 8.

Antenna installation located and installed similar to AC 43.13-2A, Chapter 3, paragraph 36a, b and 38b.

For continued airworthiness, aircraft is to be maintained in accordance with PHI Maintenance Program.

Compass corrected as required.

Weight and balance amended as required.

-----END-----

Additional Sheets Are Attached



US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N233PH	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Address <u>Quality Assurance 2001 SE Evangeline Thruway</u>	
		City <u>Lafayette</u> State <u>LA</u>	Zip <u>70508-2156</u> Country <u>USA</u>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic	Manufacturer
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station	HEER617E
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>THOMAS LANE Thomas Lane 7-13-2012</u>
--	---

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <u>THOMAS LANE Thomas Lane 7-13-2012</u>
--	---

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N233PH

Nationality and Registration Mark

7-13-2012

Date

**STC INSTALLATION**

Installed Automatic Crew, Passenger, and Baggage Door Opener Kits in accordance with Bell Helicopter Textron, Inc. STC SH1990SO and Installation Report numbers AA-86010, Revision L, dated April 14, 2004; AA-85038, Revision H, dated May 14, 1997; and AA-90059, Revision P, dated September 21, 2000.

Actual weight and balance complied with.

**END**

Additional Sheets Are Attached

United States of America  
Department of Transportation -- Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SH1990SO

*This certificate issued to*

Bell Helicopter Textron, Inc.  
441 Industrial Park Rd.  
Piney Flats, TN 37686

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified herein meets the airworthiness requirements of Part 6/21.23 of the Civil Air Federal Aviation Regulations.*

*Original Product -- Type Certificate Number:* H2SW  
*Make:* Bell Helicopter Textron Canada Limited  
*Model:* 206A, 206B, 206L, 206L-1, 206L-3, 206L-4, 407

*Description of Type Design Change:* Manufacture of Automatic Door Opener Kits in accordance with Aeronautical Accessories, Inc. Drawing List Report No. AA-86001, no revision, dated May 13, 1986, or later FAA approved revisions, and installation per "Installation Instructions" listed in that report.

*Limitations and Conditions:* The installer must determine whether this design change is compatible with previously approved modifications. If the holder agrees to permit another person to use this certificate to alter a product, the holder must give the other person written evidence of that permission.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* November 8, 1985

*Date reissued:* February 1, 2011

*Date of issuance:* September 3, 1986

*Date amended:* October 5, 1992, March 19, 1996,  
March 11, 1998



*By direction of the Administrator*

*John Hardie / for*  
(Signature)

James A. Richmond, Acting Manager  
Rotorcraft Certification Office  
Southwest Region

(Title)

Any alteration of this certificate is punishable by a fine of not exceeding \$1,000, or imprisonment not exceeding 3 years, or both.



**Aeronautical Accessories, Inc**

**P.O. BOX 3689**

**BRISTOL, TENNESSEE 37625-3689 U.S.A.**

**TELEPHONE: 423-538-5151**

**800-251-7094**

**TELEFAX: 423-538-8469**

**E-MAIL: sales@aero-access.com**

**TO: Petroleum Helicopters, Inc.**  
**ATTN: Lee Prejean**  
**DATE: January 31, 2007**  
**REF: Authorization Letter**

**Aeronautical Accessories, Inc. authorizes your company to install all of our "STC" products on your "fleet of aircraft".**

**Our "STC" products are listed in our product catalog.**

**Regards,**  
**James Rumley**  
**Marketing/Sales Mgr.**

A handwritten signature in black ink, appearing to read 'J. Rumley', with a long horizontal line extending to the right.



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>Address Quality Assurance 2001 SE Evangeline Thruway</b>	
		City <b>Lafayette</b> State <b>LA</b> Zip <b>70508-2156</b> Country <b>USA</b>	

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME		(As described in Item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name	<b>BELL HELICOPTER TEXTRON, INC.</b>	U.S. Certificated Mechanic		Manufacturer	
Address	<b>301 W. PONT DES MOUTON ROAD</b>	Foreign Certificated Mechanic			
City	<b>LAFAYETTE</b> State <b>LA</b>	<input checked="" type="checkbox"/> Certified Repair Station			<b>CRS B1H2300N Limited Airframe</b>
Zip	<b>70507</b> Country <b>U.S.A.</b>	<input type="checkbox"/> Certified Maintenance Organization			<b>Limited Specialized Services</b>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual 4/1/11
--	---

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector		Manufacturer		Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station		Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>B1H2300N</b>	Signature/Date of Authorized Individual <b>GRANT McDANIEL</b> 4/1/11
---	---

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: AUX FIN**  
**ORIGINAL PART NUMBER: 206-023-126-111**  
**SERIAL NUMBER: BP1108**  
**WORK ORDER NUMBER: W164932AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-020,  
REVISION C, DATED 11/4/08.**

-----**END**-----

Additional Sheets are Attached





US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020 11/30/2007	Electronic Tracking Number  For FAA Use Only
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INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series 1-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Quality Assurance 2001 SE Evangeline Thruway	
		City Lafayette	State LA
		Zip 70508-2156	Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME		(As described in Item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	BELL HELICOPTER TEXTRON, INC.	U.S. Certificated Mechanic	Manufacturer
Address	301 W. PONT DES MOUTON ROAD	Foreign Certificated Mechanic	C. Certificate No.
City	LAFAYETTE State LA	<input checked="" type="checkbox"/> Certified Repair Station	CRS B1H2300N Limited Airframe Limited Specialized Services
Zip	70507 Country U.S.A.	<input type="checkbox"/> Certified Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual 5/22/11
--	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. B1H2300N	Signature/Date of Authorized Individual GRANT McDANIEL  5/22/11
--	--

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: AUX FIN**  
**ORIGINAL PART NUMBER: 206-023-126-112**  
**SERIAL NUMBER:**  
**WORK ORDER NUMBER: W167537AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-020,  
REVISION C, DATED 11/4/08.**

-----END-----

Additional Sheets are Attached

US Department of Transportation  
Federal Aviation Administration

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

3. For FAA Use Only

The data identified herein complies with applicable airworthiness requirements and is approved for the above described aircraft subject to conformity inspection by a person authorized in FAR 43, Section 43.7.

Date OCT 03 2001 FAA Inspector: [Signature] SW-BTR-FSDO

4. Unit Identification

5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>HEER617E Airframe, PowerPlant &amp; Accessories Ltd. Radio Class 1,2,3</b>
--	--	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-23-01</b>	Signature of Authorized Individual <b>Mark Lazard</b>
-------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-23-01</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <b>Mark Lazard</b>
--	---	--

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Followed guidelines of FAA Approved Petroleum Helicopters, Inc. Electrical Specification and Standard Practices Manual.

Installed B.F. Goodrich SKY497 Skywatch system interfaced with Garmin GPS 400 system for Traffic display, accordance with PHI Drawing No. PHI-206L-12763 Option 2 Rev. IR. and PHI Picture and Sketch Book PHI-206L3-PS-0036 Rev. D.

Installation checkout, ground and flight tests are to be performed in accordance with SKYWATCH Installation Manual No. 009-10800-001 Rev. B and Garmin 400 series Pilot's Guide Addendum Part No. 190-00140-01 Rev. B and to verify electromagnetic compatibility (EMC/EMI) with previously installed equipment. EMC testing will be conducted in accordance with Skywatch Installation Manual No. 009-10800-001, Rev. B or later revision.

An approved Rotorcraft Flight Manual Supplement No. PHI-959-206L3SKY497-002 Rev. 0. dated ~~OCT 03 2001~~ or later revision is required for installation and must be carried on board when operating SKY497 SKYWATCH SYSTEM interfaced with Garmin GPS 400 system.

SKY497 SKYWATCH Pilot's Guide Part No. 009-10801-001 Copyright 1997 or later revision, and Garmin Pilot's Guide Addendum Must be on board for operating SKY497 SKYWATCH SYSTEM.

Installation made to withstand loads as per FAA Advisory Circular AC43.13-2A, Chapter 1, paragraph 2.

Electrical load of the revised system does not exceed 80% of total generated output as required by AC43.13-1B, Chapter 11, section 3 paragraph 11-30, 11-36, and section 4, paragraph 11-53.

Wiring was installed in a suitable manner for protection against fuel, oil, water, and other detrimental substances and abrasion damage as per AC43.13-2A, Chapter 2, paragraph 27a(2).

Wires and cables are supported by insulated clamps to prevent chafing according to AC43.13-1B, Chapter 11, section 11, paragraph 11-146.

Equipment located where it is sufficiently cooled and would not be a smoke hazard or ignite readily flammable parts of the aircraft, AC43.13-2A, Chapter 2, paragraph 21a and 27e.

Equipment switches and circuit breakers placarded for radio installation as required by AC43.13-2A, Chapter 2 paragraph 27c(4).

Adequate means of inspection are provided for racks, related parts, equipment, and adjacent components according to AC43.12-2A, Chapter 1, paragraph 8.

Weight and balance amended.

Compass corrected.

Additional Sheets Are Attached

**INSTRUCTIONS FOR CONTINUED AIRWORTHINESS**

This instruction for continued airworthiness gives the data that is needed to fulfill the requirements of CFR parts 23, 25, 27, 29, 33, or 35, sections 23.1529, 25.1529, 27.1529, 29.1529, 31.82, 33.4, or 35.4 as applicable.

**ITEM NAME: B. F. Goodrich SKY497 Traffic Advisory System Installation**

**1) INTRODUCTION:**

Installation of a B.F. Goodrich SKYWATCH Traffic Advisory System, Model SKY497, interfaced to a Garmin GPS 400 system. Installed in a Bell Helicopter Model 206L-3

**2) DESCRIPTION:**

The B. F. Goodrich SKY497 Traffic Advisory System consists of a receiver, including it's mounting rack, display, an antenna, associated wiring, and any related hardware. The primary function of the system is to advise the flight crew where to look for aircraft that may pose a collision threat. SK497 alerts the flight crew to nearby Transponder equipped aircraft and assist the pilot in the visual acquisition of aircraft that may represent a danger.

**3) CONTROL, OPERATION INFORMATION:**

Reference the B. F. Goodrich SKY497 Pilot's Guide , Garmin Pilot's Guide Addendum and PHI Approval Aircraft Flight Manual Supplement, dated ~~OCT 09 2001~~.

**4) SERVICING INFORMATION:**

The B. F. Goodrich SKY497 system is on condition and their is no periodic, preventive, or scheduled maintenance required for continued operation for this system.

**5) MAINTENANCE INSTRUCTIONS:**

The scheduled Maintenance program as follow:

**a) SKY497 Display. (Garmin GPS 400)**

1. Check that GPS connector is properly secured at back of mounting rack
2. Check to ensure GPS unit is properly installed and secured at the instrument console.

**CAUTION**

**Do not use cleaning solvents on the viewing face.**

3. Check face-plate for cleanliness. Wipe the viewing face using an eyeglass lens cleaner which is specified as safe for anti-reflective coating and a clean, lint-free cloth.

**b) TRC:**

1. Check that connectors are properly mated and secured.
2. Check to ensure that the hold-down knobs on the mounting tray are secured to the TRC.

**c) Antenna:**

1. Check for dents, cracks, and punctures.

**CAUTION**

**Do not paint the Antenna.**

**Do not use cleaning solvents on the antenna.**

2. Remove all dirt and grease from surface areas. Clean with a soft cloth moistened with mild soap and water.
3. Visually inspect sealant around the antenna base. Reapply sealant if required.

## 6) TROUBLE SHOOTING INFORMATION:

Reference the B. F. Goodrich SKY497 Installation Manual No. 009-10800-001 (Rev. B) Chapter 4, and PHI Wiring Diagram No. PHI-206L-12763 Option 2 (Rev. IR).

## 7) REMOVAL AND REPLACEMENT INFORMATION:

Reference the B. F. Goodrich SKY497 Installation Manual (Manual No. 009-10800-001, Rev. B, dated Sept. 28, 1998, or later), and the FAA approved SKY497 Flight Manual Supplement dated ~~OCT 03 2001~~. Should it become necessary to remove the SKY497, secured the associated cables and wiring, collar the applicable circuit breakers, placard the aircraft that the unit has been removed, revised the weight & balance and the equipment list and make a logbook entry the unit has been removed, Refer to 91.213 of Title 14 of the Code of Federal Regulations and/or the aircraft's MEL for deferment procedures.

## 8) DIAGRAMS:

SKY497 Installation Manual No. 009-10800-001 (Rev. B) Chapter 4, PHI Picture and Sketch Book No. PHI-206L-PS-0036 Rev. C and PHI Wiring Diagram No. PHI-206L-12763 Option 2 Rev. IR.

## 9) SPECIAL INSPECTION REQUIREMENTS:

Special inspection requirements are not applicable.

## 10) APPLICATION OF PROTECTIVE TREATMENTS:

Application of protective treatments are not applicable.

## 11) DATA:

Installation requirements may be found within the accepted industry practices contained within AC43.13-1B Chapter 11, and AC43.13-2A, Chapters 3 and 11.

## 12) LIST OF SPECIAL TOOLS:

Special tools are not required.

## 13) FOR COMMUTER CATEGORY AIRCRAFT:

Not applicable, this aircraft is not a commuter category aircraft.

## 14) RECOMMENDED OVERHAUL PERIODS:

There are no additional overhaul time limitations.

## 15) AIRWORTHINESS LIMITATION SECTION:

There are no additional airworthiness limitations.

## 16) REVISION:

A letter will be submitted to the local FSDO with a copy of the revised FAA form 337 and revised ICA. The FAA inspector accepts the change by signing block 3.

**END**



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**  
Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

### 3. For FAA Use Only

### 4. Unit Identification

### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
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**D.** I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC 1991</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
----------------------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 above and described on the reverse or attachments hereto was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>19 DEC 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
---	---	--

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Installed Paravion Technology, Inc. bleed air cabin heater according to  
STC SH 2728NM.

-----END-----





US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529
	Make Bell	Model 206
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	
	Address (As shown on registration certificate) Address 2001 SE Evangeline Thruway	
	City Lafayette	State LA
	Zip 70508-2156	Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic		<input type="checkbox"/> Manufacturer	
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic		HEER617E	
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station			
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>Terrill K. Melancon</i>	3-6-2012
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### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <i>Terrill K. Melancon</i>	3-6-2012
--	---	----------

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L

Nationality and Registration Mark

3-6-2012

Date

## *Structural Repair*

### **Bulkhead Frame Stiffener Repair**

Part Nomenclature:	Bulkhead Frame Stiffener	Part Number:	RH 206-033-107-056 LH 206-033-107-055
Repair Number:	<u>PHI-206L-RP-0077</u>		

Work accomplished: Removed a cracked joggled stiffener from R/H & L/H rollover bulkheads Sta. 146.19, W.L.72.00 as required. Fabricated & installed a .032" thick 7075-T6 aluminum stiffener with a .040" thick 7075-T6 aluminum shim to R/H & L/H Rollover Bulkheads using materials listed in repair # PHI-206L-RP-0077.

Structural repairs made according to the manufacturer's helicopters structural repair manual, except the size of this repair exceeded the limits as specified in the manual.

Approval received for this repair according to data approved on FAA Form 8110-3.

By DER:	<u>M.T. Cox</u>	Designation Number:	<u>DERT-710057-SW</u>
Dated:	<u>7-28-2003</u>	8110-3 Serial Number:	<u>8110-03379</u>

Weight and Balance Negligible.

**END**

Additional Sheets Are Attached



U.S. Department of Transportation

Federal Aviation Administration

# MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <i>Bell</i>	Model <i>206 L-3</i>
	Serial No. <i>51529</i>	Nationality and Registration Mark <i>N3116L</i>
2. Owner	Name (As shown on registration certificate) <i>PHI, INC</i>	Address (As shown on registration certificate) <i>2001 SE Evangeline Thruway Lafayette, La. 70508-2156</i>

### 3. For FAA Use Only

### 4. Unit Identification

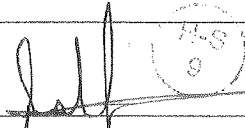
### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				
POWERPLANT	Rolls Royce	C30P/350	CAE-895845 CAT-97772	X	
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

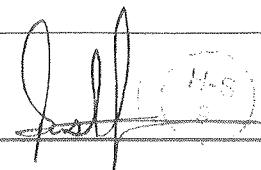
A. Agency's Name and Address <b>H-S Tool &amp; Parts Inc. #140-2560 Simpson Road Richmond, BC, V6X 2P9, Canada</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>39-87</b>
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>04/Jul/2006</b>	Signature of Authorized Individual <b>J. Gustafson</b> 
----------------------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>04/Jul/2006</b>	Certificate or Designation No. <b>39-87</b>	Signature of Authorized Individual <b>J. Gustafson</b> 		

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Description: Rolls Royce C30 Exhaust Collector  
P/N 23032325, S/N 42275

1. Weld Repaired Cracks IAW 14W3 72-50-00 Page 331 & PRPL 2-D008 Item A.
2. Installed 8 Patches IAW 14W3 72-50-00 Page 331 & PRPL 2-D008 Item C.
3. Weld Repaired 2 Struts IAW 14W3 72-50-00 Page 331 & PRPL 2-D008 Item G.
4. Cold Worked Dents & Blended Chaffing Marks IAW 14W3 72-50-00 Page 331.
5. Weld Repaired Bearing Bore IAW RSS 2402.
6. Weld Repaired Pilot Dia. "L" IAW 14W3 72-00-00 Pg. 402 Para. 1K & PRPL 2-D008 Item A.
7. Plasma Sprayed Mounting Feet Surface "A" IAW 14W3 72-50-00 Page 332 Item 9 & PRPL 2-D008 Item D(1) & PRPL 1-3 Item 18.
8. Final Machined Pilot Dia "L" IAW 14W3 72-50-00 Pg. 332 Item 10.
9. Final Machined Mounting Feet Surface "A" IAW 14W3 72-50-00 Page 332 Item 9.
10. Plasma Sprayed Rear Pilot Dia. IAW 14W3 72-50-00 Page 333 Item 17 & PRPL 1-3 Item 18.
11. Plasma Sprayed Rear Flange Surface "AN" IAW 14W3 72-50-00 Page 333 Item 15 & PRPL 1-3 Item 18.
12. Final Machined Rear Pilot Dia. IAW 14W3 72-50-00 Page 333 Item 17.
13. Final Machined Rear Flange Surface "AN" IAW 14W3 72-50-00 Page 333 Item 15.
14. Final Ground Bearing Bore IAW RSS 2402.
15. NDT (FPI) IAW 14W3 72-00-00 Page 310 Item 2B.
16. Final Inspection.

Repair Salvage Scheme (RSS) 2402 Is OEM Approved Data.

All Records of Work Performed Are On File Under Work Order 43729.

No Further Statements to Follow.

Additional Sheets Are Attached



## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>BELL</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>US N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PETROLEUM HELICOPTERS, INC.</b>	Address (As shown on registration certificate) <b>2001 EUANGELINE THRUWAY LAFAYETTE, LA 70508-2156</b>

### 3. For FAA Use Only

4. Unit Identification				5. Type		
Unit	Make	Model	Serial No.	Repair	Alteration	
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~					
POWERPLANT	Rolls Royce	250-C30P	CAE 895559 CAT 97904	X		
PROPELLER						
APPLIANCE	Type					
	Manufacturer					

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> H-S Tool & Parts Inc. #140-2560 Simpson Road Richmond, B.C. V6X 2P9 Canada	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b>  <div style="text-align: center; font-size: 1.2em;">39-87</div>
---	---	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>July 12, 2002</b>	Signature of Authorized Individual <b>J. Gustafson</b>  
------------------------------	---

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>July 12, 2002</b>		Certificate or Designation No. <b>39-87</b>	Signature of Authorized Individual <b>J. Gustafson</b>  	

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Description: Rolls Royce 250 C30 Exhaust Collector  
P/N 23032325, S/N 42277

1. Weld Repaired Cracks IAW 14W3 72-50-00 Pg. 331 & DOIL 33 Item A.
2. Installed 5 (Five) Patches IAW 14W3 72-50-00 Pg. 331 & DOIL 33 Item C.
3. Weld Repaired Struts IAW 14W3 72-50-00 Pg. 331 & DOIL 33 Item G.
4. Coldworked Dents & Blended Chaffing Marks IAW 14W3 72-50-00 Pg. 331.
5. Weld Repaired, Plasma Sprayed And Final Ground Bearing Bore IAW RSS 2402, Log# 02-1076.
6. Weld Repaired, Plasma Sprayed And Final Machined Pilot Dia. "L" IAW DOIL 33 Item A.
7. Plasma Sprayed And Final Machined Mounting Feet Surface "A" IAW 14W3 72-50-00, Pg. 332 Item 9 & OIL 3 Item 18.
8. Plasma Sprayed And Final Machined Rear Flange Surface "AN" IAW 14W3 72-50-00, Pg. 333 Item 15 & OIL 3 Item 18.
9. Plasma Sprayed And Final Machined Rear Pilot Dia. IAW 14W3 72-50-00 Pg. 333, Item 17 & OIL 3 Item 18.
10. NDT (FPI) IAW 14W3 72-00-00 Pg. 308.
11. Final Inspection.

Repair Salvage Scheme (RSS) 2402 Is OEM Approved Data.

All Records Of Work Performed Are On File Under Work Order 28691.

No Further Statements to Follow.

# NOT INSTALLED



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**  
Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>BELL</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>US N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Petroleum Helicopters, Inc.</b>	Address (As shown on registration certificate) <b>2001 SE EVANGELINE THRUWAY LAFAYETTE, LA 70508-2156</b>

**3. For FAA Use Only**

**4. Unit Identification**

Unit	Make	Model	Serial No.	5. Type	
				Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				
POWERPLANT	Rolls Royce	250-C30P	CAE 895559 CAG 95565	X	
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> H-S Tool & Parts Inc. #140-2560 Simpson Road Richmond, B.C. V6X 2P9 Canada	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b>  <div style="text-align: center; font-size: 1.2em;">39-87</div>
---	---	---

**D.** I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>May 31, 2002</b>	Signature of Authorized Individual <b>J. Gustafson</b>
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**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

<b>BY</b>	FAA Fit Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify):
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection: <b>May 31, 2002</b>		Certificate or Designation No. <b>39-87</b>	Signature of Authorized Individual <b>J. Gustafson</b>	

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Description: Rolls Royce 250 C30 Gearshaft

P/N 6889157, S/N PHI-0004

1. Chrome Plated And Final Ground Seal Journals IAW 14W3 72-00-00 Pg. 410, Final Size: 0.438" & 0.875".
2. NDT (MPI) IAW ASTM E1444.
3. Final Inspection.

All Records Of Work Performed Are On File Under Work Order 28587.

No Further Statements to Follow.

# NOT INSTALLED





## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
**For FAA Use Only**  
Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <p style="text-align: center;">Bell</p>	Model <p style="text-align: center;">206L-3</p>
	Serial No. <p style="text-align: center;">51529</p>	Nationality and Registration Mark <p style="text-align: center;">N3116L</p>
<b>2. Owner</b>	Name (As shown on registration certificate) <p style="text-align: center;">Fleet Credit Corporation</p>	Address (As shown on registration certificate) 111 Westminster Street Providence, RI 02903

### 3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
---	---	---

**D.** I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <p style="font-size: 1.2em;">19 DEC. 1991</p>	Signature of Authorized Individual <p style="font-size: 1.2em; text-align: right;"><i>James B. Edwards</i> <b>EDWARDS</b></p>
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### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <p style="font-size: 1.2em;">19 DEC. 1991</p>	Certificate or Designation No. HEER617E	Signature of Authorized Individual <p style="font-size: 1.2em; text-align: right;"><i>James B. Edwards</i> <b>EDWARDS</b></p>
--	--	--

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed a cargo net to restrain cargo in the hat rack area--Station 148.

See PHI Drawings 206L-507, sheets 1,2, and 3 of 3.

The installation is similar to AC 43.13-2A Figure 12.5 and was made according to AC 43.13-2A paragraph 243b. Picked up existing hardware points on aircraft.

The installation was load tested according to AC 43.13-2A, paragraph 246b.

The weight and balance was computed and equipment list amended as necessary.

Installed placard on hat rack as follows:

PULL RED STRAPS TO  
RELEASE CARGO NET  
MAX. WT. ALLOWED 45 LBS.

-----END-----

**NOT  
INSTALLED**

Additional Sheets Are Attached



US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

### 3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
---	---	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC. 1991</b>	Signature of Authorized Individual  <b>EDWARDS</b>
-----------------------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>19 DEC. 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual  <b>EDWARDS</b>
--	---	--

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Installed Perkins Plastic Composite baggage door according to STC SH3511SW.

-----END-----

Additional Sheets Are Attached

<p style="text-align: center;"><b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b></p> <p>US Department of Transportation Federal Aviation Administration</p>	<p>Form Approved OMB No. 2120-0020</p> <p style="text-align: center;">For FAA Use Only</p> <p>Office Identification</p>
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**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell Helicopters</b> Serial No. <b>51529</b>	Model <b>206L-3</b> Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas FL21          New York, NY 10036-8701</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>..... (As described in item 1 above) .....</i>				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement		
<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> <b>HEER617E</b> <b>Airframe, PowerPlant &amp; Accessories Ltd.</b> <b>Radio Class 1,2,3</b>

**D.** I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

<b>Date</b> 3-2-02	<b>Signature of Authorized Individual</b> (9029)
-----------------------	---

7. Approval for Return to Service					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
<b>BY</b>	FAA Fit. Standards Inspector		Manufacturer	Inspection Authorization.	Other (Specify)
	FAA Designee	X	Repair Station	Person Approved by Transport Canadian Airworthiness Group	

<b>Date of Approval or Rejection</b> 3-2-02	<b>Certificate or Designation No.</b> <b>HEER617E</b>	<b>Signature of Authorized Individual</b> (9029)
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### CONCORDE LEAD ACID BATTERY

Installed Concorde RG222 Valve Regulated Sealed Lead-Acid Battery in accordance with Falcon Crest Master Drawing List FC-003, Revision IR dated February 29, 1996 or later FAA approved revision in accordance with STC SR09069RC.

Attached Rotorcraft Flight Manual Supplement dated May 6, 1996 or later RFMS. See Flight Manual for Placard Indicating Lead-Acid Battery installed.

Weight and Balance Amended.

*END*

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N233PH</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>

<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>		Address (As shown on registration certificate) <b>Quality Assurance 2001 SE Evangeline Thruway</b>	
			City <b>Lafayette</b>	State <b>LA</b>
			Zip <b>70508-2156</b>	Country <b>USA</b>

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address			B. Kind of Agency		
Name <b>PHI, Inc.</b>	Address <b>P. O. Box 90808</b> City <b>Lafayette</b> State <b>LA</b> Zip <b>70509-0808</b> Country <b>USA</b>		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer	
Address			<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.  <b>HEER617E</b>	
City			<input checked="" type="checkbox"/> Certificated Repair Station		
Zip			<input type="checkbox"/> Certificated Maintenance Organization		

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <b>Thomas Lane</b> <i>Thomas Lane</i> <b>7-2-2012</b>
--	--

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<b>X</b>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>HEER617E</b>	Signature/Date of Authorized Individual <b>Thomas Lane</b> <i>Thomas Lane</i> <b>7-2-2012</b>
---	--

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N233PH

Nationality and Registration Mark

7-2-2012

Date

**DC Power Cable Replacement**

Replaced main DC power cables in Bell 206L-3 VFR electrical system configuration, from Battery Relay aft to Starter/Line Control Relay. Reference PHI Drawing Number PHI-206L-12641, Revision IR, 08/06/98.

Approval for this modification received on FAA Form 8110-3 completed by DER Britt Hanks, Designation Number SW-730, dated 9-22-98, serial number SW-730-98-20.

Compass corrected as required.

Actual weight and balance complied with.

**END**

Additional Sheets Are Attached



U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION <b>STATEMENT OF COMPLIANCE WITH THE FEDERAL AVIATION REGULATIONS</b>	<b>DATE</b> September 22, 1998
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AIRCRAFT OR AIRCRAFT COMPONENT IDENTIFICATION			
MAKE	MODEL NO.	TYPE (Airplane, Radio, Helicopter, etc.)	NAME OF APPLICANT
Bell Helicopter Textron	206L-1, 206L-3, 206L-4	Helicopter	Petroleum Helicopters, Inc. (PHI)

IDENTIFICATION	TITLE
PHI-206L-12641 Revision (IR), 08/06/98	<u>Drawing</u> - REPLACEMENT OF MAIN DC POWER CABLES IN 206L1, 206L3 & 206L4
PHI-206L-TA-0001 Revision (0), 05/26/98	<u>Analysis</u> - ELECTRICAL ANALYSIS FOR REPLACEMENT OF MAIN DC POWER CABLES IN A BELL HELICOPTERS MODEL 206L-1, -3, -4
Notes: <ol style="list-style-type: none"> <li>1. The <u>Electrical</u> aspects of the above listed data are approved. This approval is for the design data listed herein and not the workmanship or installation. The <u>Structural / Mechanical</u> aspects are minor; No other aspects apply.</li> <li>2. The data listed herein incorporates the intent of <u>Alert Service Bulletin (A.S.B.) No. 206L-88-50</u> dated 1/18/88; and is in agreement with the applicable portions of <u>A.S.B. No. 206L-81-20</u> through revision "B" dated 3/10/82.</li> <li>3. This approval is <u>valid only for helicopters having VFR electrical system configurations</u>, with the following effectivity:                             <ol style="list-style-type: none"> <li>a) Model 206L-1 serial number 45154 and subsequent.</li> <li>b) Model 206L-3 serial number 51001 and subsequent.</li> <li>c) Model 206L-4 serial number 52001 through 52080.</li> </ol> </li> <li>4. Additional <u>Instructions for Continued Airworthiness (ICA)</u> are not required; existing approved PHI Aircraft Maintenance Inspection Program, and Bell Helicopter Textron Maintenance Manual data are adequate for the modifications listed herein.</li> <li>5. <u>Aircraft Interior Compliance Inspection</u> is not required.</li> </ol>	

**PURPOSE OF DATA**  
 Engineering design data in support of "Major Alterations"; for Repair Station (Petroleum Helicopters, Inc. located at Lafayette, LA) return to service of Bell Model 206L-1, 206L-3, and 206L-4 helicopters.

**APPLICABLE REQUIREMENTS (List specific sections)**  
FAR Part 27, amendment 27-1 through 27-30: 27.1301(a),(b),(c); 27.1309(a),(c),(d); 27.1351(a)(1)(2); 27.1365(a),(b).

**CERTIFICATION** - Under authority vested by direction of the Administrator and in accordance with conditions and limitations of appointment under Part 183 of the Federal Aviation Regulations, data listed above and on attached sheets numbered NONE have been examined in accordance with established procedures and found to comply with applicable requirements of the Federal Aviation Regulations.

I  Therefore  Recommend approval of these data  
 Approve these data

SIGNATURE(S) OF DESIGNATED ENGINEERING REPRESENTATIVE(S)	DESIGNATION NUMBER(S)	CLASSIFICATION(S)
Britt A. Hanks	SW-730	Systems and Equipment (Electrical), Management



US Department  
of Transportation  
**Federal Aviation  
Administration**

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>2001 SE Evangeline Thruway Lafayette, LA 70508-2156</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	————— (As described in Item 1 above) —————			<b>X</b>	
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.
<b>PHI, Inc. P. O. Box 90808 Lafayette, LA 70509</b>		<input type="checkbox"/>	U.S. Certificated Mechanic	<b>HEER617E Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2 and 3</b>
		<input type="checkbox"/>	Foreign Certificated Mechanic	
		<input checked="" type="checkbox"/>	Certificated Repair Station	
		<input type="checkbox"/>	Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>11-25-'06</b>	Signature of Authorized Individual 
--------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>11-25-'06</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 
---	---	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

# Structural Repair

## Repair of Aft. Lower Shell Assembly

Part Nomenclature:	<u>Shell Assembly</u>	Part Number:	<u>206-033-099-223</u>
Repair Number:	<u>PHI-206L-RP-0115</u>	Serial Number:	<u>N/A</u>

Work performed: (Top side repair) Cut out damage area from top side of shell assy. as required. Replaced middle stiffener P/N 151-028A50204 using adhesive listed. Installed aluminum filler .020 thick with a .032 thick overlay aluminum doubler to upper side of Aft. lower shell using adhesive and NAS9303B-4 rivets. Applied fiberglass overlays to inner repair as required.

(Bottom side repair at fwd. edge) Cut out area from bottom side of shell assy. as required. Installed a .032 thick aluminum filler with a .040 thick aluminum doubler overlay at the fwd. edge of the shell assy. using adhesive and NAS9303B-4 rivets. Applied fiberglass overlays at both bottom repair areas as required.

(Bottom side repair at boost pump area) Cut out damaged area from shell assy. as required. Replaced stiffener P/N 151-028A50204 using adhesive listed. Installed a .020 thick aluminum filler with a .032 thick aluminum doubler overlay using adhesive and NAS9303B-4 rivets.

Structural repairs made according to the manufacturer's helicopters structural repair manual, except the size of this repair exceeded the limits as specified in the manual.

Approval received for this repair according to data approved on FAA Form 8110-3.

By DER	<u>Louis Bellott</u>	Designation Number	<u>DERT-750020-SW</u>
Dated	<u>11-18-06</u>	8110-3 Serial Number	<u>8110-3 LEB PHI06-27</u>

Weight and balance amended.

**END**

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate)	Address (As shown on registration certificate)
	<b>Safeco Credit Company, Inc.</b>	<b>Safeco Plaza Seattle, WA 98185</b>

### 3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b>  <b>Petroleum Helicopters, Inc.</b> P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b>  <b>Airframe, Powerplant, &amp; Accessories Limited</b> Radio Class 1, 2, & 3 CRS No. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>11-6-93</b>	Signature of Authorized Individual <i>Carly Boel</i>
------------------------	---

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>11-6-93</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>Carly Boel</i>	

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Installed door actuator on left hand, forward cabin door and baggage compartment door according to STC SH1503NM.

----- **END** -----

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

**INSTRUCTIONS:** Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>		Address (As shown on registration certificate) Address <b>2001 SE Evangeline Thruway</b>
			City <b>Lafayette</b> State <b>LA</b> Zip <b>70508-2156</b> Country <b>USA</b>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

<b>A. Agency's Name and Address</b>		<b>B. Kind of Agency</b>		<b>C. Certificate No.</b>	
Name	<b>LARRY GIBSON</b>	<input checked="" type="checkbox"/> U. S. Certificated Mechanic	Manufacturer		<b>587269291 AP/11A</b>
Address	<b>325 N. WHITWOOD AVE</b>	<input type="checkbox"/> Foreign Certificated Mechanic			
City	<b>BROOKHAVEN</b> State <b>MISS</b>	<input type="checkbox"/> Certificated Repair Station			
Zip	<b>39601</b> Country <b>USA</b>	<input type="checkbox"/> Certificated Maintenance Organization			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <div style="text-align: center;">   <b>LARRY GIBSON</b>  <b>11-16-09</b> </div>
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>587269291 AP/11A</b>	Signature/Date of Authorized Individual <div style="text-align: center;">   <b>LARRY GIBSON</b>  <b>11-16-09</b> </div>
---	--

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L

Nationality and Registration Mark

11-16-09

Date

**STC Installation**

Installed Elastomeric Tail Rotor Bearing in accordance with STC SR09248RC.

Weight and Balance amended.



Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

# MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>Address Quality Assurance 2001 SE Evangeline Thruway</b>	
		City <b>Lafayette</b> State <b>LA</b>	Zip <b>70508-2156</b> Country <b>USA</b>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	<b>ACADIAN COMPOSITES</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address	<b>301 W. PONT DES MOUTON ROAD</b>	<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City	<b>LAFAYETTE</b> State <b>LA</b>	<input checked="" type="checkbox"/> Certified Repair Station	<b>CRS B1H2300N Limited Airframe</b>
Zip	<b>70507</b> Country <b>U.S.A.</b>	<input type="checkbox"/> Certified Maintenance Organization	<b>Limited Specialized Services</b>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual 7/15/10
--	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>B1H2300N</b>	Signature/Date of Authorized Individual <b>GRANT McDANIEL</b> 7/15/10
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: ELEVATOR**

**ORIGINAL PART NUMBER: 206-023-119-159**

**UPGRADED TO PART NUMBER: 206-023-119-179**

**SERIAL NUMBER: H22436**

**WORK ORDER NUMBER: W153028AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH ACADIAN COMPOSITES  
FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-032, REVISION A, DATED  
11/24/99 AND UPGRADED IN ACCORDANCE WITH OVERHAUL PROCEDURE PHI-450-OP-050  
DATED 10/15/02, REVISION E.**

-----**END**-----

Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC 1991</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
----------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	<input type="checkbox"/>	Manufacturer	<input type="checkbox"/>	Inspection Authorization Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	<input type="checkbox"/>	

Date of Approval or Rejection <b>19 DEC 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed Emergency Float Electrical System according to STC SH1715SW.

ADDITIONAL SHEETS ARE ATTACHED

US Department of Transportation <b>Federal Aviation Administration</b>	<b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b>	Form Approved OMB No. 2120-0020
		<b>For FAA Use Only</b>
		Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Petroleum Helicopters, Inc.</b>	Address (As shown on registration certificate) <b>2001 SE Evangeline Thruway          Lafayette, La. 70508</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> <b>Petroleum Helicopters, Inc.</b> <b>2001 SE Evangeline Thruway</b> <b>Lafayette, La. 70508</b>	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> <b>HEER617E</b> <b>Airframe, Powerplant &amp; Accessories Ltd.</b> <b>Radio Class 1, 2, 3.</b>
---	---	---

**D.** I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>3-14-04</b>	Signature of Authorized Individual 
---------------------	--

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

<b>BY</b>	FAA Fit. Standards Inspector		Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canadian Airworthiness Group	
Date of Approval or Rejection <b>14 MAR 2004</b>		Certificate or Designation No. <b>HEER617E</b>		Signature of Authorized Individual 	

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**Apical Float Installation**

**Installed emergency float / life raft system in accordance with Apical Industries, Inc. STC SR01535LA.**

**Removed light weight float system and wiring previously installed in accordance with Bell SI 206-2033.**

**Installed Apical Industries, Inc. Rotorcraft Flight Manual Supplement FMS-PHI-1, Rev. A, dated 10-24-03 in flight manual as required.**

**Actual weight and balance complied with.**

----- E N D -----

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

**MAJOR REPAIR AND ALTERATION**  
(Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

1. Aircraft	Nationality and Registration Mark <i>N233PH</i>	Serial No. <i>51529</i>
	Make <i>Bell</i>	Model <i>206E</i> Series <i>L-3+</i>
2. Owner	Name (As shown on registration certificate) <i>PHI Inc</i>	Address (As shown on registration certificate) Address <i>2001 SE Evangeline Thruway</i> City <i>Lafayette</i> State <i>LA</i> Zip <i>70508-2156</i> Country <i>USA</i>

3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input checked="" type="checkbox"/>	<input type="checkbox"/>	POWERPLANT	<i>Rolls Royce</i>	<i>250 C-30P</i>	<i>CAE-89511</i> <i>CAI-97848</i>
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type Manufacturer		

6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <i>Cadorath Aerospace Inc.</i>	Address <i>2070 Logan Avenue</i>	City <i>Winnipeg</i> State <i>Manitoba</i>	Zip <i>R2R 0H9</i> Country <i>Canada</i>
		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
		<input checked="" type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
		<input type="checkbox"/> Certificated Repair Station	
		<input type="checkbox"/> Certificated Maintenance Organization	<i>86-91</i>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>B. Schellenberg</i> <b>MAR 15 2012</b>	CAI 03
--	--	--------

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector	Manufacturer	<input checked="" type="checkbox"/> Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <i>86-91</i>	Signature/Date of Authorized Individual <i>B. Schellenberg</i> <b>MAR 15 2012</b>	CAI 03
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US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N3116L</u>	Serial No. <u>51529</u>	
	Make <u>Bell</u>	Model <u>206</u>	Series <u>L-3</u>
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHI, Inc.</u>	Address (As shown on registration certificate) <u>2001 SE Evangeline Thruway</u>	
		City <u>Lafayette</u>	State <u>LA</u>
		Zip <u>70508-2156</u>	Country <u>USA</u>

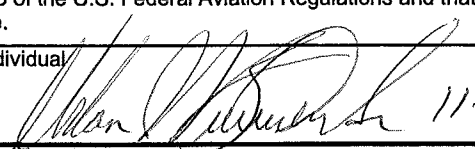
### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

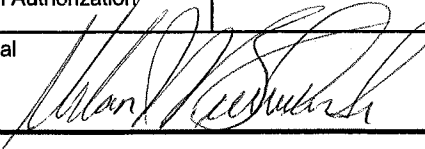
A. Agency's Name and Address		B. Kind of Agency	
Name <u>PHI, Inc.</u>	Address <u>P. O. Box 90808</u> City <u>Lafayette</u> State <u>LA</u> Zip <u>70509-0808</u> Country <u>USA</u>	<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
		<input checked="" type="checkbox"/> Certificated Repair Station	<b>HEER617E</b>
		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>Urban J. Bienvenu Sr.</u>  <u>11-30-2010</u>
---	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<b>X</b>	Repair Station	Inspection Authorization	Other (Specify)
Certificate or Designation No. <b>HEER617E</b>			Signature/Date of Authorized Individual <u>Urban J. Bienvenu Sr.</u>  <u>11-30-2010</u>		



**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L  
Nationality and Registration Mark

12-27-10  
Date

**STRUCTURAL REPAIRS**

<b>Part Nomenclature:</b>	<b>Exhaust Stack</b>	<b>P/N:</b>	<b>206-064-300-005</b>
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Structural repairs made according to Petroleum Helicopters, Inc. Engineering Department Process Specification Document Number PHI-905-SS-001 Revision 1, dated 5/17/05.

Approval received for these repairs according to data approved on FAA Form 8110-3 by DER Louis Bellot, Designation Number DERT-750020-SW, dated July 5, 2005, serial number LEB 05-075

Weight And Balance Negligible.



Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas Fl 21 New York, NY 10036-8701</b>

**3. For FAA Use Only**

The data identified herein complies with applicable airworthiness requirements and is approved for the above described aircraft subject to conformity inspection by a person authorized in FAR 43, Section 43.7.

Date: 5/5/99 FAA Inspector: [Signature] SW-BTR-FSDO

**4. Unit Identification**

**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station Manufacturer:	<b>C. Certificate No</b> Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2 and 3 - CRS No. Heer617E
---	---	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>02-17-00</b>	Signature of Authorized Individual <u>[Signature]</u> 5567
-------------------------	---

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>02-17-00</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <u>[Signature]</u> 5567
--	---	---

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Installed external transporter tube according to PHI Drawing PHI206L1-532 Sheets 1 through 7.

Installation previously approved on 337 dated 10-16-98 on A/C N31801.

Flight manual supplement as required by FAR133.47 completed and carried in Rotorcraft Flight Manual.

Weight and balance amended as necessary.

~~XX~~ Additional Sheets Are Attached

N3116L S/N 51529 MODEL 206L-3

INSTRUCTIONS FOR CONTINUED AIRWORTHINESS

This instruction for continued airworthiness gives the data that is needed to fulfill the requirements of CFR parts 23, 25, 27, 29, 33, or 35, sections 23.1529, 25.1529, 27.1529, 29.1529, 31.82, 33.4, or 35.4 as applicable.

ITEM NAME

EXTERNAL CARGO TRANSPORTER TUBE

1) INTRODUCTION:

Installed external transporter tube made of PVC pipe attaching to the landing gear cross tubes and AFT jack point to carry radio antennas and other long objects.

2) DESCRIPTION:

The transporter tube is a 3" PVC pipe 181 1/2" long mounted to the cross tube support with a stainless steel clamping arrangement on the left side of the aircraft. It has a removal cap on the AFT end to facilitate loading. The cap is safetified by a cable and safety pin. Total weight is 13.8 pounds.

3) CONTROL, OPERATION INFORMATION:

Remove cap on AFT end of tube to load or unload as required. Secure cap and safety with safety pin.

4) SERVICING INFORMATION:

N/A

5) MAINTENANCE INSTRUCTIONS:

Inspect for security, corrosion and general condition of the tube and attaching parts in accordance with Petroleum Helicopters, Inc Maintenance Program or FAA Annual.

6) TROUBLE SHOOTING INFORMATION:

N/A

7) REMOVAL AND REPLACEMENT INFORMATION:

Install and remove in accordance with PHI Drawing PHI 206L1-532.

8) DIAGRAMS:

N/A

9) SPECIAL INSPECTION REQUIREMENTS:

N/A

10) APPLICATION OF PROTECTIVE TREATMENTS:

None

11) DATA:

PHI Drawing 206L1-532

12) LIST OF SPECIAL TOOLS:

None

13) FOR COMMUTER CATEGORY AIRCRAFT:

A. Electrical loads

N/A

B. Methods balancing flight controls.

N/A

C. Identification of primary and secondary structures

N/A

D. Special repair methods applicable to the airplane

N/A

14) RECOMMENDED OVERHAUL PERIODS:

No additional overhaul time limitations.

15) AIRWORTHINESS LIMITATION SECTION:

None

16) REVISION:

A letter will be submitted to the local FSDO with a copy of the revised FAA form 337 and revised ICA. The FAA inspector accepts the change by signing Block 3.

END



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Safeco Credit Company, Inc.</b>	Address (As shown on registration certificate) <b>Safeco Plaza Seattle, WA 98185</b>

### 3. For FAA Use Only

The data identified herein complies with applicable airworthiness requirements and is approved for the above described aircraft subject to conformity inspection by a person authorized in FAR 43, Section 43.7.

Date: **MAY 05 1994** FAA Inspector: *[Signature]* SW-BTR-FSDO

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certified Mechanic <input type="checkbox"/> Foreign Certified Mechanic <input checked="" type="checkbox"/> Certified Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2, and 3 - CRS No. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>5-7-94</b>	Signature of Authorized Individual <i>[Signature]</i>
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### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>5-7-94</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>[Signature]</i>
--	---	--

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### *External Oil Filters*

Removed engine external oil filter, associated lines, and hardware previously installed according to FAA Form 337 dated 19 Dec 1991.

Installed filter assembly, P/N 1740001, by Facet Filter Products Division according to Petroleum Helicopters, Inc., Drawing Number 206L-431.

This filter assembly is the same unit installed on 206 Series aircraft according to STC SH200GL.

For maintenance instructions concerning this installations, see Petroleum Helicopters, Inc., FAA Approved FAR 135 Maintenance Manual.

Decal required on forward side of aft firewall, PHI-105D-58.

- A. Inspect red button on bottom of bowl for indication of filter bypass.
- B. If red button is not showing proceed with preflight.
- C. Red button showing, reset button once and run engine.
- D. If red button reappears, discontinue operations and investigate reason for filter bypass indication.

Weight and balance amended accordingly.

-----  
**END**  
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Additional Sheets Are Attached



US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

### 3. For FAA Use Only

### 4. Unit Identification

### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
---	---	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC. 1991</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
-----------------------------	---

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>19 DEC. 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
--	---	---



## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed fire extinguisher on aft side of vertical tunnel, fuselage station 83. See PHI drawing number PHI 360-206L, sheet 1.

Fire extinguisher and bracket is mounted with existing aircraft vertical tunnel bolts and made to withstand loads as per AC 43.13-2A Chapter 1, Paragraph 2 Table of Limits.

Materials and fasteners used conform to AC 43.13-2A, Chapter 1, paragraphs 4 and 6.

Adequate means of inspection are provided for rack, related parts, equipment and adjacent components according to AC 43.13-2A Chapter 1, paragraph 8.

Weight & balance amended as required and equipment list amended according to AC 43.13-2A, Chapter 1, paragraph 9. Total weight 4 lbs.

-----END-----

# NOT INSTALLED

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Address 2001 SE Evangeline Thruway	
		City Lafayette State LA	Zip 70508-2156 Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.  HEER617E
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station	
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>William D. Smith</i> <span style="float: right;">7-5-09</span>
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <i>William D. Smith</i> <span style="float: right;">7-5-09</span>
--	--

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L

Nationality and Registration Mark

7-05-09

Date

**Floor Protector STC Installation**

Installed crew floor protectors in accordance with Aeronautical Accessories, Inc. STC SH3289SO installation report number AA-92067, Revision D, dated June 21, 2000.

Weight and balance amended .

END

Additional Sheets Are Attached

US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

Unit	Make	Model	Serial No.	5. Type	
				Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~			X	
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
<b>Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA. 70509</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>HEER617E Airframe, Powerplant &amp; Accessories Ltd. Radio Class 1,2,3</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-29-99</b>	Signature of Authorized Individual 
-------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector		Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	X	Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-29-99</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 
--	---	--

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

***Structural Repairs***

***Part Nomenclature:*** FWD. LOWER SHELL ASSY ***P/N:*** 206-033-100-291

***206L-3 S/N 51529 ACFT. T.T. 9969:00***

Structural repairs made according to the Manufacturer's Helicopters Structural Repair Manual, except the size of some repairs exceeded the limits as specified in the manual.

Approval received for these repairs according to data approved on Form AE-100-1 by DER A. Gendron, Designation Number DB022. Dated 10-27-99, Serial Number A8585. For Field Repair Number 99RL-112.

***END***

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>Quality Assurance 2001 SE Evangeline Thruway</b>	
		City <b>Lafayette</b>	State <b>LA</b>
		Zip <b>70508-2156</b>	Country <b>USA</b>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	<b>BELL HELICOPTER TEXTRON, INC.</b>	U.S. Certificated Mechanic	Manufacturer
Address	<b>301 W. PONT DES MOUTON ROAD</b>	Foreign Certificated Mechanic	C. Certificate No.
City	<b>LAFAYETTE</b> State <b>LA</b>	<input checked="" type="checkbox"/> Certified Repair Station	<b>CRS B1H2300N Limited Airframe Limited Specialized Services</b>
Zip	<b>70507</b> Country <b>U.S.A.</b>	<input type="checkbox"/> Certified Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>[Signature]</i> <b>2/21/12</b>
--	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	Other (Specify)
Certificate or Designation No. <b>B1H2300N</b>		Signature/Date of Authorized Individual <b>GRANT McDANIEL</b> <i>[Signature]</i> <b>2/21/12</b>			

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: FORWARD SHELL  
ORIGINAL PART NUMBER: 206-033-100-291  
SERIAL NUMBER: BP000007  
WORK ORDER NUMBER: W176946AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-053,  
REVISION A, DATED 10/10/00.**

-----**END**-----

Additional Sheets are Attached

For FAA Use Only

Office Identification

US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopter</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas Fl 21 New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

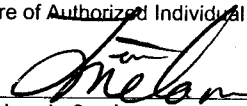
5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement


A. Agency's Name and Address <b>Acadian Composites, LLC 301 W. Pont Des Mouton Lafayette, La. 70507</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>CRS NO. HEER617E Airframe, Powerplant, Accessories Limited Class 1, 2, and 3</b>
--	--	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>8-15-01</b>	Signature of Authorized Individual 
------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canadian Airworthiness Group	
Date of Approval or Rejection <b>8-15-01</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <b>Jimmie Melancon</b> 		



**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**Nomenclature: Forward Shell**

**Original Part Number: 206-033-100-261**

**Upgraded Part Number: 206-033-100-291**

**Serial Number: BP0000165**

**Work Order Number: J0172663**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH PETROLEUM HELICOPTERS, INC. D.B.A. ACADIAN COMPOSITES, L.L.C. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-053, DATED 10/10/00, REVISION A AND UPGRADED IN ACCORDANCE WITH OVERHAUL PROCEDURE PHI-450-OP-050, DATED 1/11/01, REVISION D.**

----- E N D -----

Additional Sheets Are Attached



US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N233PH</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>		
	Address (As shown on registration certificate) Address <b>Quality Assurance 2001 SE Evangeline Thruway</b>		
	City <b>Lafayette</b>		State <b>LA</b>
	Zip <b>70508-2156</b>	Country <b>USA</b>	

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <b>PHI, Inc.</b>		<input type="checkbox"/> U. S. Certificated Mechanic <input type="checkbox"/> Manufacturer	
Address <b>P. O. Box 90808</b>		<input type="checkbox"/> Foreign Certificated Mechanic      C. Certificate No.	
City <b>Lafayette</b> State <b>LA</b>		<input checked="" type="checkbox"/> Certificated Repair Station	
Zip <b>70509-0808</b> Country <b>USA</b>		<input type="checkbox"/> Certificated Maintenance Organization	
HEER617E			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>Aaron W. Begnaud</i> 7-12-12
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>HEER617E</b>	Signature/Date of Authorized Individual <i>Aaron W. Begnaud</i> 7-12-12
---	--

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L  
Nationality and Registration Mark

7-12-12  
Date

**Garmin GDL 69/69A XM Satellite Datalink Interfaced to GNC420W GPS**

No equipment removed for this installation.

Installed a Garmin GDL69/69A XM Satellite Datalink interfaced to a Garmin GNC-420W GPS in accordance with PHI-206L-13122, Revision A. Installation was approved on FAA Form 8110-3, dated July 9, 2012, Serial No. DERT-710121-SW-12/185, by DER Verl Herd, Designation No. DER-710121-SW.

Work performed meets the requirements of Rotorcraft Airworthiness Standards for 14 CFR Part 27 of the following paragraphs:

- 27.1301 Equipment installed performs its intended function
- 27.1309(c) Equipment installed is not a hazard in the event of a malfunction
- 27.1321 Arrangement and visibility
- 27.1351 Electrical loads does not exceed generator capacity
- 27.1357 Circuit device protection
- 27.1365 Electric cable type and load
- 27.1547 Compass system tested and corrected
- 27.1581(b) Flight Manual Supplement provided if required

Equipment list updated to reflect installation of this system.

Actual weight and balance complied with.

Compass system tested and corrected.

Electrical load does not exceed 80% generator output.

Pilot's Guide addendum number 190-00356-30, Revision H, or later approved revision is required for this installation and must be carried on board when operating the Garmin GDL 69 XM Satellite Weather Datalink.

**END**

Additional Sheets Are Attached





US Department of Transportation  
Federal Aviation Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

1. Aircraft	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
2. Owner	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Address 2001 SE Evangeline Thruway	
		City Lafayette	State LA
		Zip 70508-2156	Country USA

**3. For FAA Use Only**

"The data identified herein complies with the applicable airworthiness requirements and is approved for the above described aircraft, subject to conformity inspection by a person authorized in § 43.7"

Date: 2/25/12 FAA Inspector: [Signature] SW-BTR-FSDO

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name	PHI, Inc.	<input type="checkbox"/>	U. S. Certificated Mechanic	Manufacturer	
Address	P. O. Box 90808	<input type="checkbox"/>	Foreign Certificated Mechanic		
City	Lafayette State LA	<input checked="" type="checkbox"/>	Certificated Repair Station	HEER617E	
Zip	70509-0808 Country USA	<input type="checkbox"/>	Certificated Maintenance Organization		

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>[Signature]</u> <u>AARON W BEGNAUD</u> <u>7-12-12</u>
--	---

**T. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Flt. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <u>[Signature]</u> <u>AARON W BEGNAUD</u> <u>7-12-12</u>
--	---

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L	7-12-12
Nationality and Registration Mark	Date

**Garmin GNC 420W System Interfaced with L-3 Comm SKY497 Skywatch and KCS 55A Compass System**

- Removed Garmin GPS 400 system for this installation.
- Installed a Garmin GNC 420W GPS interfaced to a L-3 Communications SKY497 Skywatch in accordance with PHI Electrical Diagrams. PHI-206L-12908, Rev. B, PHI-206L-12763, Rev. B, and PHI Picture & Sketch PHI-206L-PS-0104, Rev. IR.

- Installed the following Avionics components:

<u>MFG</u>	<u>Type</u>	<u>Description</u>	<u>Station/Location</u>	<u>Circuit Breaker</u>
Garmin	GNC 420W	GNC NAV/VHF/GPS	41.00/Center console	5 Amp
Garmin	GA-56	GPS Antenna	85.25/Servo Cowling	N/A
Comant	CI121	VHF Antenna	65.00/Servo Cowling	N/A
King	KI525A	HSI	39.00 / Inst Panel	N/A

- All Avionics circuit breakers are located in the aircraft manufacturer's supplied panels and are appropriately labeled. All wire used is MIL-W-22759 and MIL-W-27500 interconnect wire.
- These components were previously installed on a PA-32 in accordance with STC SA00705WI.

- These avionics installations are in accordance with the manufacturer's installation instructions as follows:

<u>MFG</u>	<u>Type</u>	<u>Installation Manual Number</u>	<u>Revision</u>
Garmin	400W Series	190-00356-02	G
L-3 Communications	SKY497	009-10800-001	G
Allied Signal	KCS 55A Compass System	006-00111-0007	11

- All work accomplished in accordance with:

A.C. 43.13-1B	CHAPTER 11	SECTION 3-6	Electrical loading and protection.
	CHAPTER 11	SECTION 8-17	Wire routing, marking and connecting.
	CHAPTER 10	SECTION 1-2	Weight and balance considerations.
A.C. 43.13-2B	CHAPTER 1	PARA 106 - 111	Structural mounting data.
	CHAPTER 2	PARA 202 - 206	Equipment physical mounting techniques.
PHI ESM-1	CHAPTER 3	PARA 305 - 310	Antenna physical mounting techniques.
	CHAPTER 1		Helicopter Electrical Specification and
	THRU 12		Standard Practices Manual

- Work performed meets the requirements of Rotorcraft Airworthiness Standards for 14 CFR Part 27 of the following paragraphs:

27.1301	Equipment installed performs its intended function
27.1309(c)	Equipment installed is not a hazard in the event of a malfunction
27.1321	Arrangement and visibility
27.1351	Electrical loads does not exceed generator capacity
27.1357	Circuit device protection
27.1365	Electric cable type and load
27.1547	Compass system tested and corrected
27.1581(b)	Flight Manual Supplement provided if required

- Equipment list updated to reflect installation of this system.

- Actual weight and balance complied with.

Additional Sheets Are Attached

11. Compass system tested and corrected.
12. Electrical load tested and found to be within limits.
13. Installation of GPS meets requirements of AC20.138, paragraph 7. GPS is interfaced to a KI-525A HSI and is for VFR use only. A placard is installed on the instrument panel in clear view of the Pilot which states "**GPS limited to VFR use only.**"
14. An approved flight manual supplement No. PHI-959-206L3GNC420W-001 Revision 1 dated 2/25/12 or later revision is required for this installation and must be carried on board when operating the Garmin GNC 420W interfaced with the B.F. Goodrich Skywatch system.
15. Flight check was made on 7-24-12 to assure that the accuracy requirements of AC20.138, paragraph 6 were met during flight (X) YES ( ) NOT APPLICABLE.
16. Installation checkout, ground and flight tests are to be performed in accordance with Garmin GNC-420 Pilot's guide and Garmin 400 series Pilot's guide addendum. For additional installation details see PHI Electrical Diagram no. PHI-206L-12908, Revision B, PHI-206L-12763, Rev. B, and PHI Picture & Sketch PHI-206L-PS-0104, Rev. IR.
17. Instructions for continued airworthiness PHI-915-206L-ICA-0018, Revision IR, dated February 2, 2012 are attached.

END

US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

### 3. For FAA Use Only

The data identified herein complies with applicable airworthiness requirements and is approved for the above described aircraft subject to conformity inspection by a person authorized in FAR 43, Section 43.7.

Date OCT 03 2001

FAA Inspector:

SW-BTR-FSDO

### 4. Unit Identification

### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>..... (As described in item 1 above) .....</i>				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	B. Kind of Agency <input type="checkbox"/> U. S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>HEER617E Airframe , PowerPlant &amp; Accessories Ltd. Radio Class 1,2,3</b>
--	---	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-23-01</b>	Signature of Authorized Individual 
-------------------------	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-23-01</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Followed guidelines of FAA Approved Petroleum Helicopters, Inc. Electrical Specification & Standard Practices Manual.

Installed Garmin GPS 400 system interfaced with B.F. Goodrich SKY497 Skywatch system for traffic display in accordance with PHI drawing No. PHI-206L-12762 Option 2 Rev. IR. PHI-206L-12763 Option 2 Rev. IR and PHI Picture and Sketch Book PHI-206L3-PS-0036 Rev. D. Garmin GPS 400 GPS System previously approved on STC. SA00800WI issued to GARMIN International of July 06, 1999. Garmin GNS 430 VHF Transceiver/VOR/ILS Receiver/GPS Receiver with B.F. Goodrich SKY497 Skywatch system previously approved on STC. SA00705WI issued to GARMIN International. On October 29, 1999.

An approved Rotorcraft Flight Manual Sup. No. PHI-959-206L3GPS400-002 Rev. 0 dated OCT 03 2001 or later revision is required for installation and must be carried on board when operating Garmin GPS 400 system that is interfaced with B.F. Goodrich SKY497 Skywatch system.

Garmin GPS 400 Pilot's Guide Part No. 190-00140-60 Rev. B or later version and Pilot's Guide addendum Part No. 190-00140-10 Rev. B or later version Must be on board for operating Garmin GPS 400 system that is interfaced with B.F. Goodrich SKY497 Skywatch system..

Installation made to withstand loads as per FAA advisory circular AC43.13-2A, Chapter 1, paragraph 2.

Electrical load of the revised system does not exceed 80% of total generated output as required by AC43.13-1B, chapter 11, section 3, paragraph 11-36.

Wiring was installed in a suitable manner for protection against fuel, oil, water, and other detrimental substances and abrasion damage, AC43.13-2A, Chapter 2, paragraph 27a(2).

Wires and cables are supported by insulated clamps to prevent chafing according to AC43.13-1B, Chapter 11, section 11, paragraph 11-146.

Equipment located where it is sufficiently cooled and would not be a smoke hazard or ignite readily flammable parts of the aircraft, AC43.13-2A, Chapter 2, paragraph 21a and 27e.

Equipment switches and circuit breakers placarded for radio installation as required by AC43.13-2A, Chapter 2, paragraph 27c(4).

Adequate means of inspection are provided for racks, related parts, equipment, and adjacent components according to AC43.13-2A, Chapter 1, paragraph 8.

Antenna installation located and installed similar to AC43.13-2A, Chapter 3, paragraph 36a, b and 38b. Antenna located at fuselage station 95.00 on the servo cowl.

Installation of GPS meets requirements of AC20.138, paragraph 7. GPS is interfaced to a KI-525A HSI and is for VFR use only. Installed a placard on instrument panel in clear view of the pilot which states, "GPS Limited to VFR Use Only."

Flight check was made on 10-25-01 to assure that the accuracy requirements of AC20.138, paragraph 6 were met during flight  YES { } NOT APPLICABLE

Compass corrected as required.

Weight and balance amended as required.

Additional Sheets Are Attached

**INSTRUCTIONS FOR CONTINUED AIRWORTHINESS**

This instruction for continued airworthiness gives the data that is needed to fulfill the requirements of CFR parts 23, 25, 27, 29, 33, or 35, sections 23.1529, 25.1529, 27.1529, 29.1529, 31.82, 33.4, or 35.4 as applicable.

**ITEM NAME    Garmin GPS 400**

- 1) INTRODUCTION: Installation of an Garmin GPS 400 interfaced to a King KI-525A HSI indicator installed in a Bell Helicopter Model 206L-3
- 2) DESCRIPTION: System consist of a Garmin GPS 400 Receiver, a GPS Ant. that is mounted on the servo cowling at Sta. 95.00 and the GPS Receiver is interfaced to a King KI-525A HSI indicator.
- 3) CONTROL, OPERATION INFORMATION: See Garmin GPS 400 Pilots' Guide Part No. 190-00140-60 (Rev. B) and Pilot's Guide Addendum Part No. 190-00140-10. Rev. B or later revision.
- 4) SERVICING INFORMATION: N/A
- 5) MAINTENANCE INSTRUCTIONS: N/A
- 6) TROUBLE SHOOTING INFORMATION: See PHI Wiring Diagrams PHI-206L-12762 (Rev. IR) Option 2, PHI-206L-12763 (Rev. IR) Option 2 and PHI-206L-12423 (Rev. IR)
- 7) REMOVAL AND REPLACEMENT INFORMATION: Reference the Garmin GPS 400 Installation Manual (Manual No. 190-00140-02 Rev. H) or later version.
- 8) DIAGRAMS: PHI-206L-12762 (Rev. IR Option 2) Garmin GPS 400 interfaced to a King KI-525A HSI, PHI-206L-12763 (Rev. IR Option 2) SKY497 Skywatch interfaced to Garmin GPS 400 and PHI-206L-12423 (Rev. IR) KCS 55A Compass System.
- 9) SPECIAL INSPECTION REQUIREMENTS: N/A.
- 10) APPLICATION OF PROTECTIVE TREATMENTS: N/A
- 11) DATA: N/A
- 12) LIST OF SPECIAL TOOLS: N/A
- 13) FOR COMMUTER CATEGORY AIRCRAFT: N/A
- 14) RECOMMENDED OVERHAUL PERIODS: N/A
- 15) AIRWORTHINESS LIMITATION SECTION: N/A
- 16) REVISION: A letter will be submitted to the local FSDO with a copy of the revised FAA form 337 and revised ICA. The FAA inspector accepts the change by signing block 3.

**END**



US Department of Transportation  
Federal Aviation Administration

**MAJOR REPAIR AND ALTERATION**  
(Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

1. Aircraft	Nationally and Registration Mark <b>N233PH</b>	Serial No. <b>51529</b>		
	Make <b>Bell Helicopters</b>	Model <b>206</b>	Series <b>L-3</b>	
2. Owner	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>2001 S.E. Evangeline Thruway</b>		
		City <b>Lafayette</b>	State <b>La.</b>	Zip <b>70508-2156</b>

3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input type="checkbox"/>	AIRFRAME		(As described in Item 1 above)	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	POWERPLANT	Rolls Royce	250 C30P	CAE-895558 CAG-95592
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <b>H-S Tool &amp; Parts Inc.</b>	Address <b>#140-2560 Simpson Road</b>	<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
City <b>Richmond</b>	State <b>B.C.</b>	<input checked="" type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
Zip <b>V6X 2P9</b>	Country <b>Canada</b>	<input type="checkbox"/> Certificated Repair Station	<input type="checkbox"/> Certificated Maintenance Organization
			<b>39-87</b>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <b>C. Trsek</b>	<b>Nov.22/13</b>
--	--	------------------

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	<input checked="" type="checkbox"/> Persons Approved by Canadian Department of Transport
	FAA Designee	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>39-87</b>	Signature/Date of Authorized Individual <b>C. Trsek</b>	<b>Nov.22/13</b>
--	--	------------------

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N233PH	10/06/2014
Nationality and Registration Mark	Date

Description: Rolls Royce Gearbox Housing  
P/N 23064603, S/N PC19837

1. Repaired Engine Mounts IAW RSS 1012 And E.O.128.
2. Repaired Oil Pump Drive Seal Bore And Flange IAW RSS 1051.
3. Repaired PT Tachometer Governor Locating Bore And Flange IAW RSS 1051.
4. Final Inspection.

Repair Salvage Schemes (RSS) 1012 And 1051 Are OEM Approved Data.

All Records Of Work Performed Are On File Under Work Order 73925.  
No Further Statements To Follow.

Additional Sheets Are Attached

# MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

US Department of Transportation  
Federal Aviation Administration

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

### 3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>As described in item 1 above</i>				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>HEER617E Airframe, PowerPlant &amp; Accessories Ltd. Radio Class 1,2,3</b>
--	--	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-14-2001</b>	Signature of Authorized Individual 
---------------------------	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	X	Repair Station	
Date of Approval or Rejection <b>10-14-2001</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 	

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### Installed a cargo net to restrain cargo in the hat rack area at Station 148

Installed Cargo Net to restrain cargo in the hat rack area, see PHI Drawing PHI-206-60174, Rev. B, dated 8-27-98.

This alteration is in accordance with data previously approved on FAA form 337, dated June 02,2000 for Bell model 206L-3, N49EA, serial number 51507, and is approved for duplication on identical make, model, and altered configuration when accomplished by the original modifier.

The installation is similar to AC 43.13-2A, figure 12.5 and was made according to AC 43.13-2A, paragraph 243b. Picked up existing hardware points on aircraft.

The installation was load tested according to AC 43.13-2A, paragraph 246b for up to 45 lb. load. See PHI Structural Static Load Test Document Number PHI-206-ST-0001.

For fabric burn test see PHI Report Document PHI-060-BT-0001.

The weight and balance was amended as necessary.

Installed placard on hat rack as follows:

**PULL RED STRAPS TO RELEASE  
CARGO NET  
MAX. WT. ALLOWED 45 LBS.**

#### INSTRUCTIONS FOR CONTINUED AIRWORTHINESS

This instruction for continued airworthiness gives the data that is needed to fulfill the requirements of CFR parts 23, 25, 27, 29, 33, or 35, sections 23.1529, 25.1529, 27.1529, 29.1529, 31.82, 33.4, or 35.4 as applicable.

#### **ITEM NAME: HAT RACK STORAGE BIN, CARGO RETAINING NET**

- 1) INTRODUCTION: "Hat rack" storage bin. Installation of a cargo retaining net. Addition of a web net to restrain cargo in the hat rack, which is located above the aft rear seats. Maximum load is 45 pounds and will be placarded.
- 2) DESCRIPTION: Installation of a cargo retraining net mounted across the hat rack opening.
- 3) CONTROL, OPERATION INFORMATION: Secure top out board straps over threaded studs and retain with lock pins.
- 4) SERVICING INFORMATION: None require.
- 5) MAINTENANCE INSTRUCTIONS: Petroleum Helicopters Inc. Maintenance program or FAA Annual. Inspect for condition and security.

337 ATTACHMENT SHEET DATED 11-14-01 N3116L S/N 51529 MODEL 206L-3

- 6) TROUBLE SHOOTING INFORMATION: None required.
- 7) REMOVAL AND REPLACEMENT INFORMATION: Remove and install in accordance with drawing PHI-206-60174.
- 8) DIAGRAMS: None required.
- 9) SPECIAL INSPECTION REQUIREMENTS: None required.
- 10) APPLICATION OF PROTECTIVE TREATMENTS: None required.
- 11) DATA: In accordance with drawing PHI-206-60174.
- 12) LIST OF SPECIAL TOOLS: None required.
- 13) FOR COMMUTER CATEGORY AIRCRAFT: N/A
- 14) RECOMMENDED OVERHAUL PERIODS: N/A
- 15) AIRWORTHINESS LIMITATION SECTION: N/A
- 16) REVISION:  
A letter will be submitted to the local FSDO with a copy of the revised FAA form 337 and revised ICA.  
The FAA inspector accepts the change by signing block 3.

*END*



US Department of Transportation

Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Quality Assurance 2001 SE Evangeline Thruway	
		City Lafayette	State LA
		Zip 70508-2156	Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	BELL HELICOPTER TEXTRON, INC.	U.S. Certificated Mechanic	Manufacturer
Address	301 W. PONT DES MOUTON ROAD	Foreign Certificated Mechanic	C. Certificate No.
City	LAFAYETTE State LA	<input checked="" type="checkbox"/> Certified Repair Station	CRS B1H2300N Limited Airframe
Zip	70507 Country U.S.A.	<input type="checkbox"/> Certified Maintenance Organization	Limited Specialized Services

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual 9/2/11
--	---

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. B1H2300N	Signature/Date of Authorized Individual GRANT MCDANIEL  9/2/11
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## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: HORIZONTAL STABILIZER**

**ORIGINAL PART NUMBER: 206-023-119-151**

**SERIAL NUMBER: AO-04209**

**WORK ORDER NUMBER: W169149AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-052,  
REVISION A, DATED 8/30/99.**

-----**END**-----

Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N233PH</u>	Serial No. <u>51529</u>		
	Make <u>BELL</u>	Model <u>206</u>	Series <u>L-3+</u>	
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHI INC.</u>	Address (As shown on registration certificate) Address <u>2001 S.E. EVANGELINE THRUWAY</u>		
		City <u>LAFAYETTE</u>	State <u>LA</u>	Zip <u>70508-2156</u> Country <u>U.S.A.</u>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name	<u>BELL HELICOPTER TEXTRON, INC.</u>		U.S. Certificated Mechanic		Manufacturer
Address	<u>301 W. PONT DES MOUTON ROAD</u>		Foreign Certificated Mechanic		<b>CRS B1H2300N Limited Airframe Limited Specialized Services</b>
City	<u>LAFAYETTE</u> State <u>LA</u>	<input checked="" type="checkbox"/>	Certified Repair Station		
Zip	<u>70507</u> Country <u>U.S.A.</u>		Certified Maintenance Organization		

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>GM 7/26/13</u>
--	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>B1H2300N</b>	Signature/Date of Authorized Individual <u>GRANT MCDANIEL GM 7/26/13</u>
---	---

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N233PH	5-4-17
Nationality and Registration Mark	Date

**NOMENCLATURE: HORIZONTAL STABILIZER  
ORIGINAL PART NUMBER: 206-023-119-151  
SERIAL NUMBER: H17804  
WORK ORDER NUMBER: W205068AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-052,  
REVISION B, DATED 9/1/11.**

-----  
**END**  
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Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N233PH	Serial No. 51529	
	Make Bell	Model 206	Series L-3

<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.		Address (As shown on registration certificate) Quality Assurance 2001 SE Evangeline Thruway	
			City Lafayette	State LA
			Zip 70508-2156	Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station	HEER617E
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual Thomas Lane  7-13-2012
--	---

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual Thomas Lane  7-13-2012
--	---

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N233PH

7-13-2012

Nationality and Registration Mark

Date

**INCREASED GROSS WEIGHT UPGRADE**

Installed Increased Gross Weight Upgrade Kit in accordance with Bell Helicopter Service Instruction BHT-206-SI-2052 Rev. 1 dated October 14, 2010. Installed 206L4 Flight Manual with Flight Manual Supplements and Manufactures Data.

Compass tested and corrected as required.

Performed Actual Weight and Balance.

*END*

Additional Sheets Are Attached



U.S. Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved

OMB No. 2120-0020

For FAA Use Only

Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <i>Boeing</i>	Model <i>206L-3</i>
	Serial No. <i>51529</i>	Nationality and Registration Mark <i>N316L</i>
2. Owner	Name (As shown on registration certificate) <i>SAFE CO CREDIT COMPANY INC</i>	Address (As shown on registration certificate) <i>SAFE CO PLAZA SEATTLE WA 98185</i>

3. For FAA Use Only

4. Unit Identification

	Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~					X
POWERPLANT						
PROPELLER						
APPLIANCE	Type					
	Manufacturer					

6. Conformity Statement

A. Agency's Name and Address <i>Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509</i>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2, and 3 - CRS No. HEER617E
---	--	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <i>7-9-96</i>	Signature of Authorized Individual <i>Robert J. Meeker</i>
-----------------------	---

7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <i>7-9-96</i>	Certificate or Designation No. <i>HEER617E</i>	Signature of Authorized Individual <i>Robert J. Meeker</i>
--	---	---

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### Modification to Input Pinion

Transmission S/N A-33 TT 4479.50 TSO - 0 -

Installed a No. 8 neoprene plug in Transmission input pinion out board end in addition to Bell Helicopter Textron cork. This No. 8 neoprene plug is an aid to prevent oil leakage through the cork.

The Data for this modification is FAA Approved on FAA Form 8110-3 dated 5 June 1996 serial number 8110-961 by Bell Helicopter FAA DER James D. Rockwood No. SW-931.

END

**NOT  
INSTALLED**



US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

1. Aircraft	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
2. Owner	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Address Quality Assurance 2001 SE Evangeline Thruway	
		City Lafayette	State LA
		Zip 70508-2156	Country USA

**3. For FAA Use Only**

"The data identified herein complies with the applicable airworthiness requirements and is approved for the above described aircraft, subject to conformity inspection by a person authorized in § 43.7"

Date: 6/13/12 FAA Inspector: [Signature] SW-BTR-FSDO

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <u>PHI, Inc.</u>		U. S. Certificated Mechanic	
Address <u>P. O. Box 90808</u>		Foreign Certificated Mechanic	
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station	
Zip <u>70509-0808</u> Country <u>USA</u>		Certificated Maintenance Organization	
		C. Certificate No. <u>HEER617E</u>	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>Patrick P. McDonald</u> <u>PATRICK P. Mc DONALD</u> <u>06-14-2012</u>
--	---

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <u>HEER617E</u>	Signature/Date of Authorized Individual <u>Patrick P. McDonald</u> <u>PATRICK P. Mc DONALD</u> <u>06-14-2012</u>
---	---



**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L	06-14-2012
Nationality and Registration Mark	Date

**Alteration**

**Installation of Antenna Mount on Battery Door**

Fabricated and installed antenna mount on battery door in accordance with Picture and Sketch PHI-206L-PS-0112, Revision IR, dated December 7, 2011.

Installation of Battery Door Safety Clip in accordance with Drawing PHI-206-60557 Revision A. dated 12-16-2011.

Structural Analysis Report for installation of a battery door antenna mount is in accordance with PHI-206L-SA-0103 Revision IR, dated 12-16-2011.

Approval for the Structural aspects for this alteration according to data previously approved on FAA Form 8110-3 for Bell Model 206L-3, N230PH, S/N 51506.

By DER	<u>Roland M. Howard, Jr.</u>	Designation Number	<u>DERT-710134-SW</u>
Dated	<u>12-28-2011</u>	8110-3 Serial Number	<u>1112-68</u>

For Company Flight Test Plan and Results for Battery Door Antenna Mount Provisions see PHI-206L-TP-0009 Revision A dated 01-23-2012.

Approval for the flight evaluation aspects for this alteration according to data previously approved on FAA Form 8110-3 for Bell Model 206L-3, N230PH, S/N 51506.

By DER	<u>Robert A. Williams</u>	Designation Number	<u>DERT-710526-SW</u>
Dated	<u>01-23-2012</u>	8110-3 Serial Number	<u>None</u>


For Instructions for Continued Airworthiness see PHI-915-206L-ICA-0010, Revision A dated 12-24-2011.

Actual Weight and Balance performed.

**END**

Additional Sheets Are Attached

RAW-01-23-12

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION <b>STATEMENT OF COMPLIANCE WITH AIRWORTHINESS STANDARDS</b>			1. DATE 23 January, 2012
<b>AIRCRAFT OR AIRCRAFT COMPONENT IDENTIFICATION</b>			
2. MAKE Bell Helicopter Textron Canada Limited	3. MODEL NO. 206L-3	4. TYPE (Airplane, Radio, Helicopter, etc.) Helicopter	5. NAME OF APPLICANT PHI, Inc. P.O. Box 90808 Lafayette, LA 70509
<b>LIST OF DATA</b>			
6. IDENTIFICATION Report: PHI-206L-TP-0009 Rev A, Date: 23 Jan, 2012	7. TITLE Company Flight Test Plan and Results for Battery Door Antenna Mount Provisions on Bell Helicopter Helicopter Textron Canada Limited Model 206L-3 & 206L-4 Helicopters -----END OF DATA-----  Notes: 1) Only the Flight Test Pilot and Flight Test Analyst aspects of the results of the above listed data are approved herein. This approval is only for the engineering data. It indicates the data listed above demonstrated compliance with the regulations specified by paragraph and subparagraph listed below as "Applicable Requirements." 2) This form does not constitute FAA approval of all the engineering data necessary for substantiation of compliance to necessary requirements for the entire alteration. 3) This project requires approval of structural data.		
8. PURPOSE OF DATA In support of a major alteration for serial number 51506.			
9. APPLICABLE REQUIREMENTS (List specific sections) 14 CFR Part 27: 27.51(a); 27.65(a)(1); 27.75 (a)(1), 27.141(a) & (b); 27.143(a), (b) & (c); 27.161; 27.171; 27.173; 27.175; 27.177; 27.251; 27.773(a)(1); 27.1301(a), (c) & (d) and 27.1323(c) (Flight Test Pilot and Flight Test Analyst aspects only)*****			
10. CERTIFICATION - Under authority vested by direction of the Administrator and in accordance with conditions and limitations of appointment under Part 183, data listed above and on attached sheets (none) have been examined in accordance with established procedures and found to comply with applicable requirements of the Airworthiness Standards listed..  <input type="checkbox"/> Recommend approval of these data <input checked="" type="checkbox"/> Approve these data I (We) Therefore			
11. SIGNATURE(S) OF DESIGNATED ENGINEERING REPRESENTATIVE(S) Robert A. Williams 	12. DESIGNATION NUMBER(S) DERT-710526-SW	13. CLASSIFICATION(S) Flight Test Pilot & Flight Test Analyst	

U.S. Department of Transportation Federal Aviation Administration <b>STATEMENT OF COMPLIANCE WITH AIRWORTHINESS STANDARDS</b>	1. Date DEC 28 2011
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Aircraft or Aircraft Component Identification			
2. Make	3. Model No.	4. Type (Airplane, Radio, Helicopter, etc.)	5. Name of Applicant
BELL HELICOPTER TEXTRON CANADA LIMITED	206L-3	HELICOPTER	PHI, INC.

**LIST OF DATA**

6. Identification	7. Title
DOC. PHI-206L-PS-0112, REV. IR, 12-07-11	PICTURE AND SKETCH DATA FOR ANTENNA MOUNT INSTALLED ON BATTERY DOOR IN BELL MODEL 206L SERIES HELICOPTER
DWG. PHI-206-60557, REV. A, 12-16-11	206 BATTERY DOOR SAFETY CLIP
RPT. PHI-206L-SA-0103, REV. IR, 12-16-11	STRUCTURAL ANALYSIS REPORT FOR INSTALLATION OF A BATTERY DOOR ANTENNA MOUNT ON BELL HELICOPTER TEXTRON CANADA LIMITED 206L-3 & 206L-4 HELICOPTERS

**STRUCTURAL APPROVAL ONLY**

only for Bell Model 206L-3, S/N 51506.

Notes: 1. This approval is for design data only. It indicates the data listed above demonstrates compliance only with the regulations specified by paragraph listed below as "Applicable Requirements."  
 2. This form constitutes FAA approval of all the engineering data necessary for substantiation of compliance to necessary requirements for the alterations as specified in the above data.

8. Purpose of Data  
 SUPPORT OF MAJOR ALTERATIONS FOR S/N 51506.  
 THIS APPROVAL IS FOR ENGINEERING DESIGN DATA ONLY.

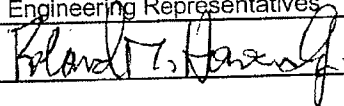
9. Applicable Requirements (List specific sections)

14 CFR Sections 27.301(a)(b)(c), 27.303, 27.305(a), 27.307(a)[3], 27.337(a), 27.561, 27.601(a), 27.603(a)(b)(c)[16], 27.605(a)[16], 27.609(a)(b), 27.613(a)(b)(c)(d)[16], 27.625(a)(c).  
 In accordance with 14 CFR Part 27 with amendments 27-1 through 27-24, exceeding the original certification basis per TCDS H2SW (CAR 6). All are at amdt. 27-0 unless noted as [\*], where \* is amdt. level.

10. CERTIFICATION - under authority vested by direction of the Administrator and in accordance with the conditions and limitations of appointment under 14 CFR Part 183, data listed above and on attached sheets numbered (none) have been examined in accordance with the established procedures and found to comply with applicable requirements of the Airworthiness Standards listed.

I (We) Therefore

Recommend approval of these data  
 Approve these data

11. Signature(s) of Designated Engineering Representatives	12. Designation Number(s)	13. Classifications
Roland M. Howard, Jr., PE 	DER-710134-SW	Structures

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION <b>STATEMENT OF COMPLIANCE WITH AIRWORTHINESS          STANDARDS</b>	1. DATE 23 January, 2012
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AIRCRAFT OR AIRCRAFT COMPONENT IDENTIFICATION			
2. MAKE  Bell Helicopter Textron Canada Limited	3. MODEL NO.  206L-3	4. TYPE (Airplane, Radio, Helicopter , etc.)  Helicopter	5. NAME OF APPLICANT  PHI, Inc. P.O. Box 90808 Lafayette, LA 70509

LIST OF DATA	
6. IDENTIFICATION  Report: PHI-206L-TP-0009 Rev A, Date: 23 Jan, 2012	7. TITLE  Company Flight Test Plan and Results for Battery Door Antenna Mount Provisions on Bell Helicopter Helicopter Textron Canada Limited Model 206L-3 & 206L-4 Helicopters -----END OF DATA-----  Notes: 1) Only the Flight Test Pilot and Flight Test Analyst aspects of the <u>results</u> of the above listed data are approved herein. This approval is only for the engineering data. It indicates the data listed above demonstrated compliance with the regulations specified by paragraph and subparagraph listed below as "Applicable Requirements." 2) This form does not constitute FAA approval of all the engineering data necessary for substantiation of compliance to necessary requirements for the entire alteration. 3) This project requires approval of structural data.

8. PURPOSE OF DATA In support of a major alteration for serial number 51506.
---

9. APPLICABLE REQUIREMENTS (List specific sections) 14 CFR Part 27: 27.51(a); 27.65(a)(1); 27.75 (a)(1), 27.141(a) & (b); 27.143(a), (b) & (c); 27.161; 27.171; 27.173; 27.175; 27.177; 27.251; 27.773(a)(1); 27.1301(a), (c) & (d) and 27.1323(c) (Flight Test Pilot and Flight Test Analyst aspects only)*****
---

10. CERTIFICATION - Under authority vested by direction of the Administrator and in accordance with conditions and limitations of appointment under Part 183, data listed above and on attached sheets (none) have been examined in accordance with established procedures and found to comply with applicable requirements of the Airworthiness Standards listed..  <input type="checkbox"/> Recommend approval of these data <input checked="" type="checkbox"/> Approve these data
--

11. SIGNATURE(S) OF DESIGNATED ENGINEERING REPRESENTATIVE(S) Robert A. Williams	12. DESIGNATION NUMBER(S) DERT-710526-SW	13. CLASSIFICATION(S) Flight Test Pilot & Flight Test Analyst
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US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make Bell	Model 206L-3
	Serial No. 51529	Nationality and Registration Mark N3116L
<b>2. Owner</b>	Name (As shown on registration certificate) Fleet Credit Corporation	Address (As shown on registration certificate) 111 Westminster Street Providence, RI 02903

**3. For FAA Use Only**

The data identified herein complies with applicable airworthiness requirements and is approved for the above described aircraft subject to conformity inspection by a person authorized in FAR 43 Section 43.7  
Date DEC 17 1991 FAA Inspector James R. Edwards SW-BTR-FSDO

**4. Unit Identification**

**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <u>19 DEC 1991</u>	Signature of Authorized Individual <u>James R. Edwards</u> <b>EDWARDS</b>
----------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <u>19 DEC 1991</u>	Certificate or Designation No. HEER617E	Signature of Authorized Individual <u>James R. Edwards</u> <b>EDWARDS</b>
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed P/N 22516, external oil filter, full flow type manufactured by Facet Enterprises, Inc. which is identical to Bell Model 205A-1, P/N 22516 oil filter.

Deactivated the electrical bypass light by removing electrical parts and installing a 1/4" 2024T3 aluminum plate and reinstalling same as original "O" rings and screws. Change filter element from original filter to porous media filter element, P/N AC9482F-2.

Filter assembly installed at Station 188 aft right side of engine compartment. Total weight of installation 4.5 pounds.

Fabricated a doubler of 301 1/4 hard .036 stainless steel. Doubler 7 3/4" wide on aft edge, 9" wide on forward edge with 1/4" L angle 5 1/4" long measuring from left to right, 5 3/4" long with 1/4" L angle full length on right side and 4 7/8" long on left side. Doubler fastened with MS20615M-3-3 & 4-4 rivets. Picked up existing row of rivets forward up two existing screws at forward left part to fit existing plate. Trimmed off L angle of existing plate.

Drilled 4 each 1/4" holes to mount filter assembly at approximately 45 degree angle to C/L of helicopter, from right side to left side & aft end forward of doubler, hole #1 2.25" X 4.1", hole #2 3.4" X 5.3", hole #3 5.3" X 1.55" and hole #4, 6.35" X 2.75".

Filter housing assembly fastened to doubler with 4 each AN4-6A bolts with a spacer made of 301 1/4 hard .050 stainless steel between filter & doubler & 4 each AN960-416L washers & AN970-4 washers & AN365-428C nuts.

Used existing hose, P/N 70-056K000V420 (Engine out to oil cooler) and attached to inlet side of external oil filter. Fabricated one new hose of aeroquip 701-10 hose measuring 10 inches long. Installed 45 degree 8846 fitting on one end of hose to firewall and 90 degree 8891 fitting on other end of hose to filter. Installed AN833-10D fitting with AN924-10 nut to inlet side of filter and AN815-10D fitting to outlet side of filter. Used same size "O" rings as original in filter. Covered hose assembly with asbestos cover same as original.

This installation previously approved on 337 dated December 27, 1983 on Bell Model 206L-3, N31801, S/N 51074.

Weight and balance computed.

-----END-----

# NOT INSTALLED

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Safeco Credit Company, Inc.</b>	Address (As shown on registration certificate) <b>Safeco Plaza Seattle, WA 98185</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b>  <b>Petroleum Helicopters, Inc.</b> <b>P.O. Box 90808</b> <b>Lafayette, LA 70509</b>	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b>  <b>Airframe, Powerplant, &amp; Accessories Limited</b> <b>Radio Class 1, 2, &amp; 3</b> <b>CRS No. HEER617E</b>
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>12-29-94</b>	Signature of Authorized Individual 
-------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Ft. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>12-29-94</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 	

### NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished  
(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### INSTALLED THE FOLLOWING EQUIPMENT

<u>MODEL</u>	<u>STATION</u>	<u>DRAWING NUMBER</u>
1. Instrument Services Clock	40.0	206L1-12003

Individual system protected by a fuse; 1.) 1/4 amp.

Installation made to withstand loads as per FAA advisory circular AC43.13-2A, Chapter 1, paragraph 2.

Battery and generator are sufficient for electrical load according to AC43.13-2A, paragraph 27d, Chapter 2.

Wiring was installed in a suitable manner for protection against fuel, oil, water, and other detrimental substances and abrasion damage, AC43.13-2A, Chapter 2, paragraph 27a(2).

Wires and cables are supported by insulated clamps to prevent chafing according to AC43.13-1A, Chapter 11, section 7, paragraph 518.

Equipment located where it is sufficiently cooled and would not be a smoke hazard or ignite readily flammable parts of the aircraft, AC43.13-2A, Chapter 2, paragraph 21a and 27e.

Adequate means of inspection are provided for racks, related parts, equipment, and adjacent components according to AC43.13-2A, Chapter 1, paragraph 8.

Compass corrected as required.

Weight and balance amended as required.

-----END-----





US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>Quality Assurance 2001 SE Evangeline Thruway</b>	
		City <b>Lafayette</b>	State <b>LA</b>
		Zip <b>70508-2156</b>	Country <b>USA</b>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	<b>BELL HELICOPTER TEXTRON, INC.</b>	<input type="checkbox"/>	U.S. Certificated Mechanic
Address	<b>301 W. PONT DES MOUTON ROAD</b>	<input type="checkbox"/>	Foreign Certificated Mechanic
City	<b>LAFAYETTE</b> State <b>LA</b>	<input checked="" type="checkbox"/>	Certified Repair Station
Zip	<b>70507</b> Country <b>U.S.A.</b>	<input type="checkbox"/>	Certified Maintenance Organization
			Manufacturer
			C. Certificate No.
			<b>CRS B1H2300N Limited Airframe Limited Specialized Services</b>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual 2/23/12
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization		Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Inspection Authorization		Other (Specify)
Certificate or Designation No. <b>B1H2300N</b>		Signature/Date of Authorized Individual <b>JASON PICARD</b> 2/23/12				

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: KICK PANEL**

**ORIGINAL PART NUMBER: 206-031-102-057**

**UPGRADED TO PART NUMBER: 206-031-102-153**

**SERIAL NUMBER: BCJ3563**

**WORK ORDER NUMBER: W183142AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-002, REVISION A, DATED 5/1/98 AND UPGRADED IN ACCORDANCE WITH OVERHAUL PROCEDURE PHI-450-OP-050 DATED 10/15/02, REVISION E.**

-----END-----

Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

### 3. For FAA Use Only

### 4. Unit Identification

### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC. 1991</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
-----------------------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>19 DEC. 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### INSTALLED THE FOLLOWING EQUIPMENT

<u>MODEL</u>	<u>STATION</u>	<u>DRAWING NUMBER</u>
1 ea. King KY196A VHF System	43.30	PHI-11368
1 ea. King KT76A Transponder System	40.00	PHI-206L1-11094
1 ea. ACK A-30 Blind Encoder System	22.00	PHI-206L1-11094
1 ea. PHI 001L Mixer Box & ICS System	48.00	PHI-11360
1 ea. RNAV 20/21 Loran System	47.54	PHI-206B/L-11181
1 ea. Avionics Master Circuit	73.00	PHI-206L3-11698
1 ea. FF-40 FM Radio System	44.00	PHI-11170
1 ea. DK100 Dukane Pinger	225.00	403-206L+B

The Dukane "pinger" is a self contained battery unit and does not use any aircraft systems. It is utilized as an aid in locating underwater wreckage of crashed aircraft.

Individual systems protected by following circuit breakers: VHF KY196 - 7.5 amp, KT76A - 3.0 amp, ACK A-30 - 3 amp, PHI mixer box/ICS - 5 amp, RNAV Loran - 5 amp, and FF 40 Radio - 5 amp.

Battery and generator are sufficient for electrical load according to AC43.12-2A, Para. 27D, Chapter 2.

Installation made to withstand loads as per AC43.13-2A, Chapter 1, Para. 2.

Wiring was installed in a suitable manner for protection against fuel, oil, water and other detrimental substances and abrasion damage AC43.13-2A, Chapter 2, Para. 27A-2.

Wires and cables are supported by insulated clamps to prevent chafing according to AC43.13-1A, Chapter 11, Section 7, Para. 518.

Equipment located where it is sufficiently cooled and would not be a smoke hazard or ignite readily inflammable parts of the aircraft. AC43.13-2A, Chapter 2, Para. 21A and 27E.

Equipment switches and circuit breakers placarded for radio installation as required AC43.13-2A, Chapter 2, Para. 27C (4).

Adequate means of inspection are provided for racks, related parts, equipment and adjacent components according to AC43.13-2A, Chapter 1, Para. 8.

Antenna installation located and installed similar to AC 43.13.2A, Chapter 3, paragraphs 36a and b and 38b. Also followed guides of paragraph 42.

Installation of Loran meets the requirements of Advisory Circular 20-121A, paragraph 7a1, 2, 3, 4, 5 and Appendix 1. Loran is a stand alone unit and for V.F.R. use only.

\* For Transponder Certification:

I certify that Transponder, Part Number KT-76A, has been tested and inspected and found to comply with (FAR 91.413 and (FAR 43 Appendix F.)

\* For Automatic Pressure Altitude Reporting Certification:

I certify that the installed Automatic Pressure Altitude Reporting System has been tested and inspected and found to comply with (FAR 91.411) and (FAR 43 Appendix E, paragraph C.)

Compass corrected as required.

Amended weight and balance as required.

-----END-----

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.		Address (As shown on registration certificate) Address 2001 SE Evangeline Thruway
			City Lafayette State LA Zip 70508-2156 Country USA

### 3. For FAA Use Only

"The data identified herein complies with the applicable airworthiness requirements and is approved for the above described aircraft, subject to conformity inspection by a person authorized in § 43.7"

Date: 2/23/12 FAA Inspector: [Signature] SW-BTR-FSDO

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name PHI, Inc.		<input type="checkbox"/> U. S. Certificated Mechanic		HEER617E	
Address P. O. Box 90808		<input type="checkbox"/> Foreign Certificated Mechanic			
City Lafayette State LA		<input checked="" type="checkbox"/> Certificated Repair Station			
Zip 70509-0808 Country USA		<input type="checkbox"/> Certificated Maintenance Organization			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>[Signature]</u> <u>AARON W BEGNAUD</u> <u>7-12-12</u>
--	---

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Flt. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <u>[Signature]</u> <u>AARON W BEGNAUD</u> <u>7-12-12</u>
--	---

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L	7-12-12
Nationality and Registration Mark	Date

**L-3 Communications SKY497 Skywatch Traffic Advisory System Interfaced to a Garmin GNC-420W GPS**

- No equipment removed for this installation.
- Installed an L-3 Communications Sky497 Skywatch interfaced to a Garmin GNC-420W GPS in accordance with PHI electrical diagrams no. PHI-206L-12763, Rev. B, PHI-206-12908, Rev. B, and PHI Picture & Sketch PHI-206L-PS-0036, Rev. G. "Option - 1" of PHI-206L-12763 is being used for this installation.

- Installed the following avionics components:

<u>MFG</u>	<u>Type</u>	<u>Description</u>	<u>Station/Location</u>	<u>C/B.</u>
L-3 Communications	TRC-497	Trans/Receiver Computer	202.00/Aft bag. Comp.	5.0 amp
L-3 Communications	NY-164	Directional Antenna	88.00/ Aft Servo Cowling	N/A

- All Avionics circuit breakers are located in the aircraft manufacturer's supplied panels and are appropriately labeled. All wire used is MIL-W-22759 and MIL-W- 27500 interconnect wire.

- These components were previously installed on an A23CE aircraft in accordance with STC SA00733CH.

- These Avionics installations are in accordance with the manufacturers installation instructions as follows:

<u>MFG</u>	<u>TYPE</u>	<u>Installation Manual Number</u>	<u>Revision</u>
L-3 Communications	SKY-497	009-10800-001	Rev. G
Garmin	400W Series GPS	190-00356-02	Rev. G

- All work accomplished in accordance with:

A.C. 43.13-1B	CHAPTER 11	SECTION 3-6	Electrical loading and protection.
	CHAPTER 11	SECTION 8-17	Wire routing, marking and connecting.
	CHAPTER 10	SECTION 1-2	Weight and balance considerations.
A.C. 43.13-2B	CHAPTER 1	PARA 106 - 111	Structural mounting data.
	CHAPTER 2	PARA 202 - 206	Equipment physical mounting techniques.
	CHAPTER 3	PARA 305 - 310	Antenna physical mounting techniques.
PHI ESM-1	CHAPTER 1		Helicopter Electrical Specification and
	THRU 12		Standard Practices Manual

- Work performed meets the requirements of Rotorcraft Airworthiness Standards for 14 CFR Part 27 of the following paragraphs:

27.1301	Equipment installed performs its intended function
27.1309(c)	Equipment installed is not a hazard in the event of a malfunction
27.1321	Arrangement and visibility
27.1351	Electrical loads does not exceed generator capacity
27.1357	Circuit device protection
27.1365	Electric cable type and load
27.1547	Compass system tested and corrected
27.1581(b)	Flight Manual Supplement provided if required

- Equipment list updated to reflect installation of this system.

- Actual weight and balance complied with.


- Compass system tested and corrected.

Additional Sheets Are Attached

11. EMC testing will be conducted in accordance with SKYWATCH Installation Manual for Qualitative EMI ground and flight test.
12. Electrical load does not exceed 80% of generator capacity.
13. An approved Rotorcraft Flight Manual Supplement No. PHI-959-206L3-SKY497-005, Revision 0 dated 2/23/12 or later revision is required for installation and must be carried on board when operating an L-3 Communications SKY497 SKYWATCH TRAFFIC ADVISORY SYSTEM.
14. Installation checkout, ground and flight tests are to be performed in accordance with L-3 Communications SKY497 Installation Manual and Garmin 400 Series Pilot's Guide Addendum. For additional installation details see PHI electrical diagram no. PHI-206L-12763, Rev. B, PHI-206-12908, Rev. B, and PHI Picture & Sketch PHI-206L-PS-0036, Rev. G.
15. Instructions for continued airworthiness PHI-915-206L-ICA-0018, Revision IR, dated February 2, 2012 are attached.

*END*

FILE

 US Department of Transportation <b>Federal Aviation Administration</b>	<b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b>	Form Approved OMB No. 2120-0020
		<b>For FAA Use Only</b>
		Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Safeco Credit Company, Inc.</b>	Address (As shown on registration certificate) <b>Safeco Plaza Seattle, WA 98185</b>

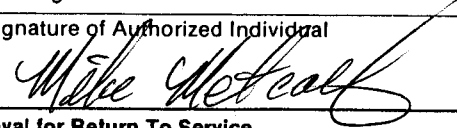
**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b>	<b>B. Kind of Agency</b>	<b>C. Certificate No.</b>
<b>Petroleum Helicopters, Inc.</b> <b>P.O. Box 90808</b> <b>Lafayette, LA 70509</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>Airframe, Powerplant, &amp; Accessories Limited</b> <b>Radio Class 1, 2, &amp; 3</b> <b>CRS No. HEER617E</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>03-15-94</b>	Signature of Authorized Individual 
-------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>03-15-94</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 	



**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**Installed Aeronautical Accessories, Inc., lightweight, replacement, nitrogen, reservoir assembly on emergency flotation system in accordance with STC SR00175AT.**

----- **END** -----

**NOT  
INSTALLED**

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N233PH</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>		
	Address (As shown on registration certificate) <b>Quality Assurance 2001 SE Evangeline Thruway</b>		
	City <b>Lafayette</b>	State <b>LA</b>	
	Zip <b>70508-2156</b>	Country <b>USA</b>	

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.  <b>HEER617E</b>
Name <b>PHI, Inc.</b>	Address <b>P. O. Box 90808</b> City <b>Lafayette</b> State <b>LA</b> Zip <b>70509-0808</b> Country <b>USA</b>	<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer	
		<input type="checkbox"/> Foreign Certificated Mechanic		
		<input checked="" type="checkbox"/> Certificated Repair Station		
		<input type="checkbox"/> Certificated Maintenance Organization		

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <b>Thomas Lane</b> <i>Thomas Lane</i> <b>6-28-2012</b>
--	---

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<b>X</b>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>HEER617E</b>	Signature/Date of Authorized Individual <b>Thomas Lane</b> <i>Thomas Lane</i> <b>6-28-2012</b>
---	---

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N233PH

6-28-2012

Nationality and Registration Mark

Date

**STC INSTALLATIONS**

1. Installed passenger door rubber mounted wedge windows in accordance with Aeronautical Accessories, Inc. STC SH5685SW and Installation Instructions Report Number AA-91040, Revision F, dated March 3, 2003.
2. Installed litter door and fixed panel rubber mounted windows in accordance with Aeronautical Accessories, Inc. STC SH7608SW and Installation Instructions Report Number AA-91047, Revision M, dated December 11, 2007.
3. Installed Rubber Mounted Crew Flat Window Assembly in accordance with Aeronautical Accessories, Inc. STC SH5773SW and Installation Instructions Report Number AA-91020, Revision F, dated August 1, 2001.

Compass corrected as required.

Actual weight and balance complied with.

**END**

Additional Sheets Are Attached

United States of America  
Department of Transportation — Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SH5773SW

*This certificate, issued to* Aeronautical Accessories, Inc.  
P.O. Box 3689  
Bristol, Tennessee 37625

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 6 of the Civil Air Regulations.*

*Original Product — Type Certificate Number:* H2SW  
*Make:* Bell Helicopter Textron, Inc.  
*Model:* 206A, 206B, 206L, 206L-1, 206L-3, 206L-4

*Description of Type Design Change:* Installation of rubber seal mounted crew door windows and/or crew door wedge windows with vent in accordance with Aeronautical Accessories, Inc. Drawing List Report No. AA-95001, no revision; dated January 9, 1995, (previously known as Heli-Plex Installation Drawing No. 26HP900, Revision C, dated August 15, 1989) or later FAA approved revision.

*Limitations and Conditions:* Compatibility of this modification with previously installed equipment must be determined by the installer.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked, or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* April 24, 1984

*Date issued:* October 26, 1990

*Date of issuance:* August 3, 1984

*Date amended:* June 23, 1989, Rev. 1,  
October 5, 1992, January 13, 1995

*By direction of the Administrator*



*Roger D. Anderson*

(Signature)  
Roger D. Anderson  
Manager, Atlanta Aircraft  
Certification Office

(Title)

*Any alteration of this certificate is punishable by a fine of not exceeding \$1,000, or imprisonment not exceeding 3 years, or both.*

*This certificate may be transferred in accordance with FAR 21.47.*



**Aeronautical Accessories, Inc**

**P.O. BOX 3689**

**BRISTOL, TENNESSEE 37625-3689 U.S.A.**

**TELEPHONE: 423-538-5151**

**800-251-7094**

**TELEFAX: 423-538-8469 E-MAIL: [sales@aero-access.com](mailto:sales@aero-access.com)**

**TO: Petroleum Helicopters, Inc.**  
**ATTN: Lee Prejean**  
**DATE: January 31, 2007**  
**REF: Authorization Letter**

**Aeronautical Accessories, Inc. authorizes your company to install all of our "STC" products on your "fleet of aircraft".**

**Our "STC" products are listed in our product catalog.**

**Regards,**  
**James Rumley**  
**Marketing/Sales Mgr.**

A handwritten signature in black ink, appearing to read 'J. Rumley', with a long horizontal flourish extending to the right.

United States of America  
Department of Transportation — Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SH7608SW

*This certificate issued to* Aeronautical Accessories, Inc.  
P.O. Box 3689  
Bristol, Tennessee 37625

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 6 of the Civil Air Regulations.*

*Original Product — Type Certificate Number:* H2SW  
*Make:* Bell Helicopter Textron, Inc.  
*Model:* 206L, 206L-1, 206L-3, 206L-4

*Description of Type Design Change:* Installation of a rubber mounted window in the litter door and/or fixed panel in accordance with Heli-Plex Drawing No. 26HP907, Revision IR, dated July 16, 1987, or later FAA approved revision.

*Limitations and Conditions:* Compatibility of this modification with previously installed equipment must be determined by the installer.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked, or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* November 2, 1987

*Date issued:* October 26, 1990

*Date of issuance:* June 15, 1989

*Date amended:* October 5, 1992



*By direction of the Administrator*  
*Paul C. Scroggins*  
PAUL C. SCROGGINS,  
Acting Manager, Atlanta Aircraft  
Certification Office

(Title)

Any alteration of this certificate is punishable by a fine of not exceeding \$1,000, or imprisonment not exceeding 3 years, or both.



**Aeronautical Accessories, Inc**

**P.O. BOX 3689**

**BRISTOL, TENNESSEE 37625-3689 U.S.A.**

**TELEPHONE: 423-538-5151**

**800-251-7094**

**TELEFAX: 423-538-8469**

**E-MAIL: sales@aero-access.com**

**TO: Petroleum Helicopters, Inc.**  
**ATTN: Lee Prejean**  
**DATE: January 31, 2007**  
**REF: Authorization Letter**

**Aeronautical Accessories, Inc. authorizes your company to install all of our "STC" products on your "fleet of aircraft".**

**Our "STC" products are listed in our product catalog.**

**Regards,**  
**James Rumley**  
**Marketing/Sales Mgr.**

A handwritten signature in black ink, appearing to be 'J. Rumley', with a long horizontal flourish extending to the right.

United States of America  
Department of Transportation — Federal Aviation Administration  
**Supplemental Type Certificate**

*Number*

SH5685SW

*This certificate, issued to*

Aeronautical Accessories, Inc.  
P.O. Box 3689  
Bristol, Tennessee 37625

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 6 of the Civil Air Regulations.*

*Original Product — Type Certificate Number:*

H2SW

*Make:*

Bell Helicopter Textron, Inc.

*Model:*

206B, 206L, 206L-1, 206L-3, 206L-4

*Description of Type Design Change:*

Installation of wedge window assemblies, Part Number 26HP901-1, -2, -3, -4 in accordance with Heli-Plex Drawing 26HP901, Sheets 1 and 2 dated December 1, 1983, Drawing HPS-105 dated December 1, 1983, and Drawing 26HP-909 dated December 1, 1983, or later FAA approved revision.

*Limitations and Conditions:*

Compatibility of this modification with previously installed equipment must be determined by the installer.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked, or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:*

January 31, 1984

*Date received:*

October 26, 1990

*Date of issuance:*

March 23, 1984

*Date amended:*

October 5, 1992

*By direction of the Administrator*

*Paul C. Sconyers*

(Signature)

Paul C. Sconyers  
Acting Manager, Atlanta Aircraft  
Certification Office

(Title)



Any alteration of this certificate is punishable by a fine of not exceeding \$1,000, or imprisonment not exceeding 3 years, or both.





**Aeronautical Accessories, Inc**

**P.O. BOX 3689**

**BRISTOL, TENNESSEE 37625-3689 U.S.A.**

**TELEPHONE: 423-538-5151**

**800-251-7094**

**TELEFAX: 423-538-8469 E-MAIL: [sales@aero-access.com](mailto:sales@aero-access.com)**

---

**TO: Petroleum Helicopters, Inc.**  
**ATTN: Lee Prejean**  
**DATE: January 31, 2007**  
**REF: Authorization Letter**

**Aeronautical Accessories, Inc. authorizes your company to install all of our "STC" products on your "fleet of aircraft".**

**Our "STC" products are listed in our product catalog.**

**Regards,**  
**James Rumley**  
**Marketing/Sales Mgr.**


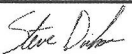
A handwritten signature in black ink, appearing to be 'JR', with a long horizontal flourish extending to the right.

UNITED STATES OF AMERICA  
DEPARTMENT OF TRANSPORTATION-FEDERAL AVIATION ADMINISTRATION  
**STANDARD AIRWORTHINESS CERTIFICATE**

1 NATIONALITY AND REGISTRATION MARKS <b>N233BH</b>	2 MANUFACTURER AND MODEL <b>Bell Helicopter Textron 206L-3</b>	3 AIRCRAFT SERIAL NUMBER <b>51529</b>	4 CATEGORY <b>NORMAL</b>
<p>5 AUTHORITY AND BASIS FOR ISSUANCE</p> <p>This airworthiness certificate is issued pursuant to 49 U.S.C. § 44704 and certifies that, as of the date of issuance, this aircraft has been inspected and found to conform to its type certificate and be in a condition for safe operation. This aircraft meets the requirements of the applicable airworthiness standards in Annex 8 to the Convention on International Civil Aviation, except as follows:</p> <p><b>None</b></p>			
<p>6 TERMS AND CONDITIONS</p> <p>Unless sooner surrendered, suspended, revoked, or a termination date is otherwise established by the FAA, this airworthiness certificate is effective as long as maintenance, preventative maintenance, and alterations are performed per the applicable Federal Aviation Regulations and the aircraft is registered in the United States.</p>			
DATE OF ISSUANCE <b>R 18 Nov 1991</b>	FAA REPRESENTATIVE <b>Wayne C. Pipkins</b>		DESIGNATION NUMBER <b>ODA-AFS-831285-SW</b>
<p>Any alteration, misuse, or reproduction of this certificate for a fraudulent purpose may be punishable by certificate revocation, fine, and / or imprisonment.</p> <p><b>THIS CERTIFICATE MUST BE DISPLAYED IN THE AIRCRAFT PER THE APPLICABLE FEDERAL AVIATION REGULATIONS.</b></p>			
<p>FAA Form 8100-2 (11-2016) Previous Edition Dated 04-11 May be Used Until Depleted</p>			

06/05/2018  
*R. Favre*

**REGISTRATION NOT TRANSFERABLE**

UNITED STATES OF AMERICA DEPARTMENT OF TRANSPORTATION – FEDERAL AVIATION ADMINISTRATION CERTIFICATE OF AIRCRAFT REGISTRATION		This certificate must be in the air- craft when operated.
NATIONALITY AND REGISTRATION MARKS <b>N 233BH</b>	AIRCRAFT SERIAL NO. 51529	
MANUFACTURER AND MANUFACTURER'S DESIGNATION OF AIRCRAFT BELL 206L-3 ICAO Aircraft Address Code: 50411706		
I S S U E D  T O	PHI AVIATION LLC 2001 SE EVANGELINE TRWY LAFAYETTE LA 70508-2156	This certificate is issued for registration purposes only and is not a certificate of title. The Federal Aviation Administration does not determine rights of ownership as between private persons.
	LLC	 U.S. Department of Transportation <b>Federal Aviation                  Administration</b>
It is certified that the above described aircraft has been entered on the register of the Federal Aviation Administration, United States of America, in accordance with the Convention on International Civil Aviation dated December 7, 1944, and with Title 49, United States Code, and regulations issued thereunder.		
DATE OF ISSUE <b>September 5, 2019</b> EXPIRATION DATE <b>September 30, 2022</b>	 <b>ADMINISTRATOR</b>	

AC Form 8050-3 (10/2010) Supersedes previous editions

CUT ON DOTTED LINE



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Quality Assurance 2001 SE Evangeline Thruway	
		City Lafayette	State LA
		Zip 70508-2156	Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	BELL HELICOPTER TEXTRON, INC.	U.S. Certificated Mechanic	Manufacturer
Address	301 W. PONT DES MOUTON ROAD	Foreign Certificated Mechanic	C. Certificate No.
City	LAFAYETTE State LA	X Certified Repair Station	CRS B1H2300N Limited Airframe Limited Specialized Services
Zip	70507 Country U.S.A.	Certified Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B

Signature/Date of Authorized Individual  
 12/9/11

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)
Certificate or Designation No. B1H2300N		Signature/Date of Authorized Individual JASON PICARD  12/9/11			

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: NOSE FAIRING**  
**ORIGINAL PART NUMBER: 206-031-124-111**  
**SERIAL NUMBER: BCSG103**  
**WORK ORDER NUMBER: W179419AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-024 DATED  
4/29/98.**

-----**END**-----

Additional Sheets are Attached



US Department  
of Transportation  
**Federal Aviation  
Administration**

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Petroleum Helicopters, Inc.</b>	Address (As shown on registration certificate) <b>2001 SE Evangeline Thruway Lafayette, LA 70508-2156</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	————— (As described in Item 1 above) —————				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>HEER617E Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2 and 3</b>
--	--	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>7-22-04</b>	Signature of Authorized Individual <i>Keith Joseph Leon</i>
------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>7-22-2004</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>Leland Jan</i>	

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### **8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

### **NOSE SHELF, AVIONICS / TIE DOWN**

Installed rack manufactured from 2024T3 aluminum sheet and aluminum angles. See PHI Drawing Number 400-206. Located rack to left hand nose of aircraft picking up existing mounting points and located others as necessary to mount rack. Secured with approved fasteners. Rack provides mounting for tie down kit, ELT, and certain radio transceivers.

This alteration is in accordance with data previously approved on FAA form 337, dated 8-19-00 for Bell model 206L-3, N206FS, serial number 51506 and is approved for duplication on identical make, model, and altered configuration when accomplished by the original modifier.

Performed load analysis in accordance with Petroleum Helicopters Load Test Document Number PHI-206-ST-0006. Installed decal indicating "Max Allowable Weight" of 40 pounds on shelf.

Structure will withstand required loads as per AC 43.13-2A, chapter 1, paragraph 2. Fabrication is consistent and compatible with aircraft structure as per AC 43.13-2A, chapter 1, paragraph 5. Materials and fasteners are all of approved types according to AC 43.13-2A, chapter 1, paragraphs 4 and 5. Parts are protected from corrosion and do not interfere with required inspection.

Equipment is located where it is sufficiently cooled and would not be a smoke hazard or ignite readily inflammable parts of the aircraft, AC 43-13-2A, chapter 2, paragraphs 21a and 27e.

Adequate means of inspection are provided for racks, related parts, equipment, and adjacent components according to AC 43.13-2A, chapter 1, paragraph 8.

Amended weight and balance as required.

Additional Sheets Are Attached

**INSTRUCTIONS FOR CONTINUED AIRWORTHINESS**

This instruction for continued airworthiness gives the data that is needed to fulfill the requirements of CFR parts 23, 25, 27, 29, 33, or 35, sections 23.1529, 25.1529, 27.1529, 29.1529, 31.82, 33.4, or 35.4 as applicable.

**ITEM NAME: NOSE SHELF, AVIONICS/TIE DOWN**

**1) INTRODUCTION:**

Installed rack in the left hand nose area of the aircraft to provide mounting for the tie down kit, ELT and certain radio transceivers.

**2) DESCRIPTION:**

Nose shelf (rack) is manufactured of 2024T3 aluminum sheet and angle. It is secured to the aircraft structure by picking up existing mounting points and locating others as necessary.

**3) CONTROL, OPERATION INFORMATION:**

N/A

**4) SERVICING INFORMATION:**

N/A

**5) MAINTENANCE INSTRUCTIONS:**

Inspect for condition and security in accordance with Petroleum Helicopters, Inc. Maintenance Program or FAA Annual.

**6) TROUBLE SHOOTING INFORMATION:**

N/A

**7) REMOVAL AND REPLACEMENT INFORMATION:**

Install and remove in accordance with PHI Drawing 400-206.

**8) DIAGRAMS:**

PHI Drawing Number 400-206.

**9) SPECIAL INSPECTION REQUIREMENTS:**

N/A

**10) APPLICATION OF PROTECTIVE TREATMENTS:**

N/A

**11) DATA:**

PHI Drawing Number 400-206 and PHI Load Test Document PHI 206-ST-0006.

**12) LIST OF SPECIAL TOOLS:**

N/A

**13) FOR COMMUTER CATEGORY AIRCRAFT:**

N/A



337 ATTACHMENT SHEET DATED <sup>7-22-04</sup> N3116L S/N 51529 MODEL 206L-3

14) RECOMMENDED OVERHAUL PERIODS:  
N/A

15) AIRWORTHINESS LIMITATION SECTION:  
N/A

16) REVISION:

A letter will be submitted to the local FSDO with a copy of the revised FAA form 337 and revised ICA. The FAA inspector accepts the change by signing block 3.

**END**

US Department of Transportation  
Federal Aviation Administration

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>HEER617E Airframe, PowerPlant &amp; Accessories Ltd. Radio Class 1,2,3</b>
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-23-01</b>	Signature of Authorized Individual <i>Mark Hazard</i>
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7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-23-01</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>Mark Hazard</i>
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**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**OUTERLINK CP-2 SATCOM COMMUNICATION SYSTEM**

Installed Outerlink CP-2 Satcom System, Formerly Newcomb Communications, Inc. in accordance with Newcomb, Inc. STC. SR00120BO and PHI Drawing no. PHI-206-60386 Rev. IR. FAA Approval Flight Manual Supplement No. D000-0076 Rev. A, approved Sept. 25, 2000, or later FAA-approved revision must be on board when operating Outerlink CP-2 Satcom System.

Compass corrected as required.

Weight and balance amended as required.



Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

**INSTRUCTIONS:** Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N233PH</u>	Serial No. <u>51529</u>	
	Make <u>BELL</u>	Model <u>206</u>	Series <u>L-3+</u>
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHI, Inc.</u>	Address (As shown on registration certificate) Address <u>2001 S.E. EVANGELINE THRUWAY</u>	
		City <u>LAFAYETTE</u> State <u>LA.</u>	Zip <u>70508-2156</u> Country <u>USA</u>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	<u>BELL HELICOPTER TEXTRON, INC.</u>	<input type="checkbox"/>	U.S. Certificated Mechanic
Address	<u>301 W. PONT DES MOUTON ROAD</u>	<input type="checkbox"/>	Foreign Certificated Mechanic
City	<u>LAFAYETTE</u> State <u>LA</u>	<input checked="" type="checkbox"/>	Certified Repair Station
Zip	<u>70507</u> Country <u>U.S.A.</u>	<input type="checkbox"/>	Certified Maintenance Organization
		<input type="checkbox"/>	Manufacturer
		C. Certificate No. <b>CRS B1H2300N Limited Airframe Limited Specialized Services</b>	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>7/15/11</u>
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### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector		Manufacturer		Maintenance Organization		Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	<input type="checkbox"/>	Inspection Authorization	<input type="checkbox"/>	Other (Specify)

Certificate or Designation No. <b>B1H2300N</b>	Signature/Date of Authorized Individual <u>7/15/11</u> <b>JASON PICARD</b>
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N233PH	5-10-17
Nationality and Registration Mark	Date

**NOMENCLATURE: ELEVATOR**

**ORIGINAL PART NUMBER: 206-023-119-179**

**SERIAL NUMBER:**

**WORK ORDER NUMBER: W170910AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-032,  
REVISION A, DATED 11/24/99.**

-----**END**-----

Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

**INSTRUCTIONS:** Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N233PH</u>	Serial No. <u>51529</u>		
	Make <u>BELL</u>	Model <u>206</u>	Series <u>L-3+</u>	
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHI INC</u>	Address (As shown on registration certificate)		
		Address <u>2001 S.E. EVANGELINE THRUWAY</u>	City <u>LAFAYETTE</u>	State <u>LA</u>
		Zip <u>70508-2156</u>	Country <u>USA</u>	

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		Manufacturer
Name	<u>ACADIAN COMPOSITES</u>	<input type="checkbox"/>	U.S. Certificated Mechanic	
Address	<u>301 W. PONT DES MOUTON ROAD</u>	<input type="checkbox"/>	Foreign Certificated Mechanic	C. Certificate No.
City	<u>LAFAYETTE</u> State <u>LA</u>	<input checked="" type="checkbox"/>	Certified Repair Station	<u>CRS B1H2300N Limited Airframe</u>
Zip	<u>70507</u> Country <u>U.S.A.</u>	<input type="checkbox"/>	Certified Maintenance Organization	<u>Limited Specialized Services</u>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>[Signature]</u> <u>9/30/10</u>
--	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>B1H2300N</b>	Signature/Date of Authorized Individual <u>GRANT McDANIEL</u> <u>[Signature]</u> <u>9/30/10</u>
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**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N233PA	5-10-17
Nationality and Registration Mark	Date

**NOMENCLATURE: ELEVATOR**  
**ORIGINAL PART NUMBER: 206-023-119-105**  
**UPGRADED TO PART NUMBER: 206-023-119-177**  
**SERIAL NUMBER: AO-00842**  
**WORK ORDER NUMBER: W157540AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH ACADIAN COMPOSITES  
FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-032, REVISION A, DATED  
11/24/99 AND UPGRADED IN ACCORDANCE WITH OVERHAUL PROCEDURE PHI-450-OP-050  
DATED 10/15/02, REVISION E.**

-----**END**-----

Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas Fl 21 New York, NY 10036-8701</b>

### 3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2, and 3 - CRS No. HEER617E
---	---	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>2-2-99</b>	Signature of Authorized Individual 
-----------------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>2-2-99</b>		Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 	



## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Installed a passenger compartment cargo pallet in accordance with Petroleum Helicopters, Inc. Drawing No. PHI-206L1-531, Rev. A, dated 2-27-92.

The data for this installation was approved on FAA Form 8110-3 dated 3-10-92, serial number 92-777-002 by DER Michelle M. Corning, Designation Number SW-777.

**END**

Additional Sheets Are Attached

For FAA Use Only

Office Identification

## MAJOR PAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

US Department  
of Transportation  
**Federal Aviation  
Administration**

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell Helicopter</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas Fl 21 New York, NY 10036-8701</b>

**3. For FAA Use Only**

**4. Unit Identification**


**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>~~~~~ (As described in item 1 above) ~~~~~</i>				<b>X</b>
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> <b>Acadian Composites, LLC</b> <b>301 W. Pont Des Mouton</b> <b>Lafayette, La. 70507</b>	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> <b>CRS NO. HEER617E</b> <b>Airframe, Powerplant,</b> <b>Accessories Limited</b> <b>Class 1, 2, and 3</b>
---	---	--


D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>8/20/01</b>	Signature of Authorized Individual 
------------------------	--

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

<b>BY</b>	FAA Fit. Standards Inspector		Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<b>X</b>	Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>8/21/01</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual  Michael Thibodeaux
---	---	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**Nomenclature: Pass Fwd Facing Seat**  
**Original Part Number: 206-033-172-117**  
**Serial Number: BP0000085**  
**Work Order Number: J0173958**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH PETROLEUM HELICOPTERS, INC. D.B.A. ACADIAN COMPOSITES, L.L.C. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-008, DATED 8/3/99, REVISION A.**

----- E N D -----

Additional Sheets Are Attached

# MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

US Department of Transportation  
Federal Aviation Administration

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

**3. For FAA Use Only**

**4. Unit Identification**

**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>~~~~~ (As described in item 1 above) ~~~~~</i>				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> <b>HEER617E</b> Airframe , PowerPlant & Accessories Ltd. Radio Class 1,2,3
--	---	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-6-2001</b>	Signature of Authorized Individual <i>Dean Hural</i>
--------------------------	---

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector		Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	X	Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-6-2001</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>Dean Hural</i>
---	---	---

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

# Structural Modification

## Passenger, Litter and Crew Door Hinge Replacement

Part Nomenclature: Hinge Part Number: None  
Repair Number: PHI-206-60333 Part Serial Number None  
Rev. B 7-17-2001

Fabricated and installed cabin door hinges in accordance with drawing PHI-206-60333.

Approval received for these repairs according to data approved on FAA Form 8110-3

By Casey Dugas Designation Number DERY-750072-SW  
Dated 08-02-2001 Serial Number 2001-37



Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529		
	Make Bell	Model 206	Series L-3	
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Address Quality Assurance 2001 SE Evangeline Thruway		
		City Lafayette	State LA	Zip 70508-2156 Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	BELL HELICOPTER TEXTRON, INC.	<input type="checkbox"/>	U.S. Certificated Mechanic
Address	301 W. PONT DES MOUTON ROAD	<input type="checkbox"/>	Foreign Certificated Mechanic
City	LAFAYETTE State LA	<input checked="" type="checkbox"/>	Certified Repair Station
Zip	70507 Country U.S.A.	<input type="checkbox"/>	Certified Maintenance Organization
			Manufacturer
			C. Certificate No.
			CRS B1H2300N Limited Airframe Limited Specialized Services

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>Grant McDaniel</i> 3/2/11
--	---

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	Other (Specify)
Certificate or Designation No. B1H2300N		Signature/Date of Authorized Individual GRANT McDANIEL <i>Grant McDaniel</i> 3/2/11			

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: PASSENGER SEAT**  
**ORIGINAL PART NUMBER: 206-033-105-013**  
**UPGRADED TO PART NUMBER: 206-033-105-117**  
**SERIAL NUMBER:**  
**WORK ORDER NUMBER: W163293AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-004,  
REVISION B, DATED 11/23/98 AND UPGRADED IN ACCORDANCE WITH OVERHAUL  
PROCEDURE PHI-450-OP-050 DATED 10/15/02, REVISION E.**

-----**END**-----

Additional Sheets are Attached



**MAJOR REPAIR AND ALTERATION**  
(Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**  
Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make Bell	Model 206L-3
	Serial No. 51529	Nationality and Registration Mark N3116L
2. Owner	Name (As shown on registration certificate) Fleet Credit Corporation	Address (As shown on registration certificate) 111 Westminster Street Providence, RI 02903

**3. For FAA Use Only**

**4. Unit Identification**

**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	B. Kind of Agency	C. Certificate No.
	<input type="checkbox"/> U.S. Certificated Mechanic	AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
<input type="checkbox"/> Manufacturer		

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date 19 Dec. 1991	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
----------------------	---

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection 19 Dec. 1991	Certificate or Designation No. HEER617E	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
---	--	---



## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Installed rack manufactured from 2024T3 aluminum sheet and aluminum angles. See PHI Drawing Number 400-206. Located rack to left hand nose of aircraft picking up existing mounting points and located others as necessary to mount rack. Secured with approved fasteners. Rack provides mounting for tie down kit, ELT, and certain radio transceivers.

Structure will withstand required loads AC 43.13-2A, Chapter 1, Paragraph 2. Fabrication is consistent and compatible with aircraft structure AC 43.13-2A, Chapter 1, Paragraph 5. Materials and fasteners all of approved types AC 43.13-2A, Chapter 1, Paragraphs 4 and 5. Parts are protected from corrosion and do not interfere with required inspection.

Equipment located where it is sufficiently cooled and would not be a smoke hazard or ignite readily flammable parts of the aircraft, AC 43.13-2A, Chapter 2, Paragraphs 21a and 27e.

Adequate means of inspection are provided for racks, related parts, equipment and adjacent components according to AC 43.13-2A, Chapter 1, Paragraph 8.

Amended weight and balance as required.

-----END-----

# NOT INSTALLED

Additional Sheets Are Attached



## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
**For FAA Use Only**  
Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

### 3. For FAA Use Only

The data identified herein complies with applicable airworthiness requirements and is approved for the above described aircraft subject to conformity inspection by a person authorized in FAR 43 Section 43.7.  
Date DEC. 17 1991 FAA Inspector *James P. Edwards* SW-BTR-FSDO

### 4. Unit Identification

### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					//
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC. 1991</b>	Signature of Authorized Individual <i>James P. Edwards</i> <b>EDWARDS</b>
-----------------------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>19 DEC. 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>James P. Edwards</i> <b>EDWARDS</b>
--	---	--

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed PHI modified Whelen Siren System Model WS295. PHI part number PHI 002-000002. See PHI Drawing Number PHI 206L-11118.

The amplifier/control was mounted at Station 19 on the underside of the Avionics/tie down rack which was previously installed on a Form 337. A bracket enclosure was manufactured at .050 2024T3 aluminum measuring 6 1/4" X 6" X 2 1/2" and riveted to the underside of the Avionics/tie down rack using MS20426AD-4 rivets. Amplifier/control is bolted to the bracket with hardware supplied by the manufacturer.

Speaker installed on forward cross tube at station 73.0 with two half clamps fabricated of 6061T6 aluminum block 4 1/8" X 3 3/8" with 2 1/4" holes. Fastened with 4 each AN4-36A bolts; AN365-428 nuts, and AN960PD416L washers. Speaker at center of cross tube and aimed in aft direction.

Power taken from main buss. Used 18 gauge MIL-W-22579/41 wire for power and ground circuit. Used 20 gauge MIL-C-27500/SM wire to speaker. Circuit protected by MS26574-7.5 circuit breaker. Placarded circuit breaker.

Completed actual weight and balance.

-----END-----

Additional Sheets Are Attached

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

US Department of Transportation  
Federal Aviation Administration

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Company, Inc.</b>	Address (As shown on registration certificate) <b>1211 AVE. of the Americas F121 New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

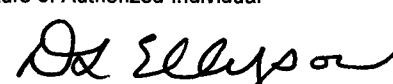
5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P. O. Box 90808 Lafayette, LA 70509</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>HEER617E Airframe , PowerPlant &amp; Accessories Ltd. Radio Class 1,2,3</b>
--	--	--


D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>12-6-01</b>	Signature of Authorized Individual 
------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>12-6-01</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 
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## NOTICE

Weight and balance or operating limitations changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed Precise Flight Pulselite System according to Precise Flight Inc. STC SH3319NM and PHI Picture and Sketch Data Document Number PHI-206L-PS-0040 Rev. IR and PHI electrical diagram no. PHI-206L-12732 Rev. IR.

Compass corrected as required.

Weight and balance amended as required.

**END**

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

**(MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance))**

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>BELL</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>US N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>PETROLEUM HELICOPTERS, INC.</b>	Address (As shown on registration certificate) <b>2001 SE EVANGELINE THRUWAY LA FAYETTE, LA 70508-2156</b>

**3. For FAA Use Only**

**4. Unit Identification**

**5. Type**

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~(As described in Item 1 above)~~~~~				
POWERPLANT	See Item 8 <b>Rolls Royce</b>	See Item 8 <b>250-C30P</b>	<b>CAE 895559</b> See item 8 <b>CAT 97904</b>	X	
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address  <b>STANDARD AERO LIMITED 33 ALLEN DYNE ROAD WINNIPEG, MANITOBA, CANADA R3H 1A1</b>	B. Kind of Agency	C. Certificate No.  <b>AMO 22-58</b>
	U.S. Certificated Mechanic	
	Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>May / 07 / 2002</b>	Signature of Authorized Individual  <b>Gary Rozmus</b>
--------------------------------	--

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  **APPROVED**  **REJECTED**

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	
Date of APPROVAL <b>May / 07 / 2002</b>		Certificate or Designation No. <b>AMO 22-58</b>	Signature of Authorized Individual  <b>Gary Rozmus</b>	

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**ROLLS-ROYCE 250/C30 P.T. SUPPORT P/N 23033412 S/N DW20294  
THE PRODUCT IDENTIFIED ABOVE WAS OVERHAULED IN ACCORDANCE WITH ROLLS-ROYCE 250/C30  
OVERHAUL MANUAL 14W3 EDITION 2 REVISION 8 DATED 01 NOVEMBER 2001, AMC-OIL 10 REVISION 12  
DATED 08 JUNE 1996 AND THE CURRENT MAINTENANCE RULES OF THE CANADIAN AVIATION  
REGULATIONS. THE PRODUCT IS APPROVED FOR RETURN TO SERVICE IN COMPLIANCE WITH CAR 571  
AND FAR PART 43.17. ALL PERTINENT DETAILS OF THE WORK PERFORMED ARE ON FILE AT THIS  
ORGANIZATION UNDER WORK ORDER L378093.**

**REFERENCE P/O: PO0074547  
T.T.S.N.: UNK T.T.S.O.: 0.0**

**SERRATED SEAL - UNMACHINED  
ABRADABLE SEALS MEACHINED 0.010" PER SIDE BELOW PRINT DIMENSIONS**

**GARY ROZMUS   
CANADIAN APPROVED INSPECTOR FOR  
STANDARD AERO LTD.  
AMO22-58**

----- **END** -----

**NOT  
INSTALLED**

Addition Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**  
Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <i>Bell</i>	Model <i>206L-3</i>
	Serial No. <i>51528</i>	Nationality and Registration Mark <i>N3116L</i>
<b>2. Owner</b>	Name (As shown on registration certificate) <i>PHI, INC</i>	Address (As shown on registration certificate) <i>2001 SE Evangeline Thruway Lafayette, La 70508-2156</i>

### 3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~			X	
POWERPLANT	<i>Rolls Royce</i>	<i>250/C-30P</i>	<i>CAE-895845 CAT-97772</i>		
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> CADORATH AEROSPACE INC. 2070 LOGAN AVENUE WINNIPEG, MANITOBA R2R 0H9	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b>  <div style="text-align: center; font-size: 1.2em;"><b>86-91</b></div>
---	---	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <i>SEP 19 2006</i>	Signature of Authorized Individual <i>G. MITCHELL</i>
----------------------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <i>SEP 19 2006</i>	Certificate or Designation No. <i>86-91</i>	Signature of Authorized Individual <i>G. MITCHELL</i>
---	--	--



### NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

#### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

REF. W/O #      PT SUPPORT      P/N# 23033412      S/N#      MK37241  
44373      250-C30

TSO:      0      TSN:      UNK      CYCLES:      UNK

- ITEM
- 1 O/H INSPECTED IAW 250 C30 14W3 O/H/M 2ND EDITION REV.12
  - 2 PLASMA SPRAY REPAIRED & FINAL MACHINED THE FOLLOWING  
IAW 250 C30 14W3 O/H/M 2ND EDITION REV.12 & PRPL 2-D002:  
NOZZLE MOUNT FACE  
FORWARD FLANGE  
SUMP FACE  
SUMP PILOT & CRUSH RING FACE
  - 3 WELD REPAIRED THE FOLLOWING IAW 250 C30 14W3 O/H/M 2ND EDITION  
REV.12 & PRPL 2-D002:  
TANGSLOTS
  - 4 CHROME PLATE REPAIRED BEARING BORE, GROOVES  
& SHOULDER IAW & PRPL 2-D002
  - 5 NDT IAW PRPL 1-8
  - 6 WELD ABOVE CIR. CLIP IAW PRPL 2-D002 & PRPL 1-4
  - 7 THERMAL SPRAY REPAIRED & FINAL MACHINED 19-24 SEALS IAW  
250 C30 14W3 O/H/M 2ND EDITION REV.12 & PRPL 2-D002
  - 8 INSTALLED NEW 25-28 SEAL P/N 23030978-1 & FINAL MACHINED IAW  
250 C30 14W3 O/H/M 2ND EDITION REV.12 AND PRPL 2-D002
  - 9 PRESSURE TESTED AND PASSED 19-24 SEAL AND CLOVERLEAF  
COVER IAW PRPL 2-D002
  - 10 CLEANED INSIDE OIL STRUTS TO REMOVE CARBON IAW  
250 C30 14W3 O/H/M 2ND EDITION REV.12
  - 11 GLASS BEAD BLAST FOR APPEARANCE IAW 14W3 O/H/M REV.12
  - 12 FINAL INSPECTION
- ADDITIONAL MAINTENANCE REQUIRED:**  
NONE

ALL RECORDS OF WORK PERFORMED ARE ON FILE UNDER W/O:      44373  
NO FURTHER STATEMENTS TO FOLLOW.

SEP 19 2006

DATE:

  
SIGNATURE:  
G. MITCHELL



STAMP

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N3116L</u>	Serial No. <u>51529</u>	
	Make <u>Bell</u>	Model <u>206</u>	Series <u>L-3</u>
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHI, Inc.</u>		Address (As shown on registration certificate) <u>Quality Assurance 2001 SE Evangeline Thruway</u>
			City <u>Lafayette</u> State <u>LA</u>
			Zip <u>70508-2156</u> Country <u>USA</u>

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic		HEER617E	
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic			
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station			
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>Patrick P. Mc Donald</u> <i>Patrick P. Mc Donald</i>	02-29-2012
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**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <u>HEER617E</u>	Signature/Date of Authorized Individual <u>Patrick P. Mc Donald</u> <i>Patrick P. Mc Donald</i>	02-29-2012
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**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L

05/09/2012

Nationality and Registration Mark

Date

## *Structural Alteration*

Part Nomenclature: **Radar Altimeter Mount Installation** Drawing Number: **PHI-206-60735 Rev. A**

Fabricated and installed provisions for the Radar Altimeter Antennas on the Tailboom at B.S. 50.22 and B.S. 71.44 in accordance with PHI-206-60735 Revision A dated 11-09-2011.

Approval received for this alteration according to data approved on FAA Form 8110-3.

By DER Louis Bellott Designation Number DERT-750020-SW

Dated 12-13-2011 Serial Number PHI11-43

Actual Weight and Balance complied with.

**END**

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.		Address (As shown on registration certificate) Address 2001 SE Evangeline Thruway
			City Lafayette State LA Zip 70508-2156 Country USA

### 3. For FAA Use Only

"The data identified herein complies with the applicable airworthiness requirements and is approved for the above described aircraft, subject to conformity inspection by a person authorized in § 43.7"

Date: 2/23/12 FAA Inspector: [Signature] SW-BTR-FSDO

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.  <b>HEER617E</b>
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic <input type="checkbox"/> Manufacturer		
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic		
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station		
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization		

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>[Signature]</u> <u>7-12-12</u>
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Flt. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>HEER617E</b>	Signature/Date of Authorized Individual <u>[Signature]</u> <u>7-12-12</u>
---	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L  
Nationality and Registration Mark

7-12-12  
Date

**RADAR ALTIMETER SYSTEM RA-4500/RAD-40 INTERFACED TO AURAL WARNING SYSTEM**

- 1. No equipment removed for this installation.
- 2. Installed a RA-4500/RAD-40 Radar Altimeter system in accordance with PHI Drawing No. PHI-206L-12910, Rev. H, dated December 5, 2011, and PHI Picture & Sketch PHI-206L-PS-0107, Rev. A, dated December 6, 2011. Radar Altimeter P/N 84560-12-300A is limited to Mod 3, or later approved revision for proper operation of the system installed.

- 3. Installed the following Avionics components:

<u>MFG</u>	<u>Type</u>	<u>Description</u>	<u>Station/Location</u>	<u>Circuit Breaker</u>
Freeflight Systems	RA-4500	RCVR/XMTR Unit	160.00 / Hatrack	2 Amp
Freeflight Systems	RAD-40	Indicator	39.00/ Instrument Panel	2 Amp
NAT	NAT-247	Audio Mixing Amp	164.00 / Hatrack	1/2 Amp
NAT	611-027	Aural Warn Gen	19.00 / Avionics Shelf	1/2 Amp
Sensor Systems	S67-2002	Antenna RX	271.00 / Tailboom	N/A
Sensor Systems	S67-2002	Antenna TX	250.00 / Tailboom	N/A

- 4. All Avionics circuit breakers are located in the aircraft manufacturer's supplied panels and are appropriately labeled. All wire used is MIL-W-22759 and MIL-W-27500 interconnect wire.
- 5. These avionics installations are in accordance with the manufacturer's installation instructions as follows:

<u>MFG</u>	<u>Type</u>	<u>Installation Manual Number</u>	<u>Revision</u>
NAT	611-027	611-16-1	1.07
NAT	NAT-247	SM247	A
Freeflight Systems	RA4500	84629	J
Freeflight Systems	RAD40	84948	B

- 6. All work accomplished in accordance with:

A.C. 43.13-1B	CHAPTER 11	SECTION 3-6	Electrical loading and protection.
	CHAPTER 11	SECTION 8-17	Wire routing, marking and connecting.
	CHAPTER 10	SECTION 1-2	Weight and balance considerations.
A.C. 43.13-2B	CHAPTER 1	PARA 106 - 111	Structural mounting data.
	CHAPTER 2	PARA 202 - 206	Equipment physical mounting techniques.
	CHAPTER 3	PARA 305 - 310	Antenna physical mounting techniques.
PHI ESM-1	CHAPTER 1 THRU 12		Helicopter Electrical Specification and Standard Practices Manual

- 7. Work performed meets the requirements of Rotorcraft Airworthiness Standards for 14 CFR Part 27 of the following paragraphs:

27.1301	Equipment installed performs its intended function
27.1309(c)	Equipment installed is not a hazard in the event of a malfunction
27.1321	Arrangement and visibility
27.1351	Electrical loads does not exceed generator capacity
27.1357	Circuit device protection
27.1365	Electric cable type and load
27.1541	Placards and Markings
27.1547	Compass system tested and corrected
27.1581(b)	Flight Manual Supplement provided if required

Additional Sheets Are Attached

8. The structural installation aspects of an RA-4500/RAD-40 Altimeter system interfaced to an aural warning system was approved on a Bell Model 206L-3, aircraft serial number 51184 on FAA Form 8110-3:

By: Louis Bellot Designation Number: DER7-750020-SW  
Dated: 12-14-2011 Serial Number: PHI11-44

9.  
10. The electrical installation aspects of an RA-4500/RAD-40 Altimeter system interfaced to an aural warning system was approved on a Bell Model 206L-3, aircraft serial number 51184 on FAA Form 8110-3:

By: Verl Herd Designation Number: DER7-710121-SW  
Dated: 12-14-2011 Serial Number: DER7-710121-SW-11/280

11. A flight test is required for the aircraft return to service for the RA-4500/RAD-40 Altimeter system interfaced to an aural warning system. PHI Flight Test Plan No. PHI-050-TP-0013, Rev. 2, dated January 23, 2012 was recommended for approval on FAA Form 8110-3:

By: Robert Williams Designation Number: DER7-710526-SW  
Dated: 01-23-2012 Serial Number: N/A

PHI-050-TP-0013 successfully completed on 7-24-12.

12. Freeflight Systems Operation/Installation Manual 84948, Rev. B, or later revision, must be carried on board when operating the RA-4500/RAD-40 Radar Altimeter system.
13. A decal will be installed on the instrument panel in lieu of a flight manual supplement to alert the pilot of the Radar Altimeter alerts and will read as follows:

**RADIO ALTIMETER VOICE ALERTS**  
**DH – “MINIMUMS, MINIMUMS”**  
**200FT AGL – “ALTITUDE, ALTITUDE”**

14. Equipment list updated to reflect installation of this system.
15. Weight and balance amended as required.
16. Compass system tested and corrected.
17. For additional installation details, refer to the following documents, PHI Drawing No. PHI-206L-12910, Rev. H, and PHI Picture & Sketch PHI-206L-PS-0107, Rev. A.
18. Instructions for continued airworthiness document PHI-915-206L-ICA-0017, Rev. IR, dated December 28, 2011 are attached.

END



**OPERATION / INSTALLATION MANUAL**

**FOR**

**FREEFLIGHT SYSTEMS**

**RAD-40 RADAR ALTIMETER DISPLAY**

**WITH PART NUMBER 84939-XX**

**FreeFlight Systems**  
**3700 Interstate 35 South**  
**Waco, TX 76706-3756 U.S.A.**  
**1 (254) 662-0000**  
**1 (800) 487-4662**



**EQUIPMENT INSTALLATION MANUAL**  
**FOR**  
**FREEFLIGHT SYSTEMS**  
**RA-4000 AND RA-4500 RADAR ALTIMETERS**

**Document No. 84629**

**Revision J**

**FreeFlight Systems**  
**3700 Interstate 35 South**  
**Waco, TX 76706-3756 U.S.A.**  
**1 (254) 662-0000**  
**1 (800) 487-4662**

October 7, 2010





# EQUIPMENT MANUAL

**Product Functional Specifications  
and Installation Manual**

**AURAL WARNING GENERATOR**

**MODEL 611-[ ]**

**AND**

**ALL DETAIL PART NUMBERS**

**NAT SEATTLE, INC.**

22125 17TH Avenue SE, Suite 107

Bothell, Washington 98021

(425) 4243-3370

FAX: (425) 424-3380

 **SEALED**

Validity  
unknown

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US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>Quality Assurance 2001 SE Evangeline Thruway</b>	
		City <b>Lafayette</b>	State <b>LA</b>
		Zip <b>70508-2156</b>	Country <b>USA</b>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	<b>BELL HELICOPTER TEXTRON, INC.</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address	<b>301 W. PONT DES MOUTON ROAD</b>	<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City	<b>LAFAYETTE</b> State <b>LA</b>	<input checked="" type="checkbox"/> Certified Repair Station	<b>CRS B1H2300N Limited Airframe Limited Specialized Services</b>
Zip	<b>70507</b> Country <b>U.S.A.</b>	<input type="checkbox"/> Certified Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual 5/28/11
--	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector	<input type="checkbox"/>	Manufacturer	<input type="checkbox"/>	Maintenance Organization	<input type="checkbox"/>	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/>	Repair Station	<input type="checkbox"/>	Inspection Authorization	<input type="checkbox"/>	Other (Specify)

Certificate or Designation No. <b>B1H2300N</b>	Signature/Date of Authorized Individual <b>JASON PICARD</b> 5/28/11
---	--

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: REAR FACING KICK PANEL**  
**ORIGINAL PART NUMBER: 206-033-113-005**  
**UPGRADED TO PART NUMBER: 206-033-113-011**  
**SERIAL NUMBER: BP0000031**  
**WORK ORDER NUMBER: W174109AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-013,  
REVISION B, DATED 2/23/99 AND UPGRADED IN ACCORDANCE WITH OVERHAUL  
PROCEDURE PHI-450-OP-050 DATED 10/15/02 REVISION E.**

-----END-----

Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N233PH</u>	Serial No. <u>51529</u>	
	Make <u>Bell</u>	Model <u>206</u>	Series <u>L-3</u>
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHI, Inc.</u>		
	Address (As shown on registration certificate) <u>Quality Assurance 2001 SE Evangeline Thruway</u>		
	City <u>Lafayette</u>	State <u>LA</u>	
	Zip <u>70508-2156</u>	Country <u>USA</u>	

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address			B. Kind of Agency		
Name <u>PHI, Inc.</u>			U. S. Certificated Mechanic		Manufacturer
Address <u>P. O. Box 90808</u>			Foreign Certificated Mechanic		C. Certificate No.  <u>HEER617E</u>
City <u>Lafayette</u> State <u>LA</u>			<input checked="" type="checkbox"/> Certificated Repair Station		
Zip <u>70509-0808</u> Country <u>USA</u>			<input type="checkbox"/> Certificated Maintenance Organization		

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>Thomas Lane</u> <u>7-13-2012</u>
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<u>X</u>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <u>HEER617E</u>	Signature/Date of Authorized Individual <u>Thomas Lane</u> <u>7-13-2012</u>
---	--

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N233PH

7-13-2012

Nationality and Registration Mark

Date

**Reconfigured Cockpit Instrument Panel Installation**

Installed Reconfigured Cockpit Instrument Panel in accordance with PHI, Inc. STC SR09594RC and PHI Master Drawing and Document List No. PHI-206L-DL-0012 Rev. B dated December 28, 2011 or later FAA approved revision.

Rotorcraft Flight Manual Supplement is Number PHI-959-206L-INST-001 Rev. O dated January 5, 2012 or later FAA approved revision.

Instructions for Continued Airworthiness are in PHI Document No. PHI-206L-ICA-0003 Rev. IR dated September 26, 2011 or later FAA accepted revision.

Compass tested and corrected as required.

Actual weight and balance complied with.

**END**

Additional Sheets Are Attached

United States of America  
Department of Transportation -- Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SR09594RC

*This certificate issued to* PHI, Inc.  
2001 SE Evangeline Thruway  
Lafayette, LA 70508-2156

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified herein meets the airworthiness requirements of Part 6 of the Civil Air Regulations.*

*Original Product -- Type Certificate Number:* H2SW  
*Make:* Bell Helicopter Textron Canada  
*Model:* 206L-3, 206L-4

*Description of Type Design Change:* Installation of Reconfiguration of the Cockpit Instrument Panel in accordance with PHI Master Drawing and Document List No. PHI-206L-DL-0012, Rev. B, dated December 28, 2011, or later FAA approved revision. Rotorcraft Flight Manual Supplement Number PHI-959-206L-INST-001, Rev. O, dated January 5, 2012, or later FAA approved revision is required. Instructions for Continued Airworthiness PHI Document No. PHI-206L-ICA-0003, Rev. IR, dated September 26, 2011, or later FAA accepted revision is required.

*Limitations and Conditions:* Compatibility of this design change with previously approved modifications must be determined by the installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* July 21, 2011

*Date reissued:*

*Date of issuance:* January 05, 2012

*Date amended:*



*By direction of the Administrator*

*Scott A. Horn*

(Signature)

Scott A. Horn, Acting Manager  
Rotorcraft Certification Office  
Southwest Region

(Title)



US Department  
of Transportation  
**Federal Aviation  
Administration**

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>2001 SE Evangeline Thruway Lafayette, LA 70508-2156</b>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	————— (As described in Item 1 above) —————			<b>X</b>	
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.
<b>PHI, Inc. P. O. Box 90808 Lafayette, LA 70509</b>		<input type="checkbox"/>	U.S. Certificated Mechanic	<b>HEER617E Airframe, Powerplant, and Accessories Limited - Radio Class 1, 2 and 3</b>
		<input type="checkbox"/>	Foreign Certificated Mechanic	
		<input checked="" type="checkbox"/>	Certificated Repair Station	
		<input type="checkbox"/>	Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>11-25-'06</b>	Signature of Authorized Individual <i>D. J. Ellyson</i>
--------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>11-25-'06</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>D. J. Ellyson</i>
---	---	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

# Structural Repair

## Repair of Aft. Lower Shell Assembly

Part Nomenclature:	<u>Shell Assembly</u>	Part Number:	<u>206-033-099-223</u>
Repair Number:	<u>PHI-206L-RP-0115</u>	Serial Number:	<u>N/A</u>

Work performed: (Top side repair) Cut out damage area from top side of shell assy. as required. Replaced middle stiffener P/N 151-028A50204 using adhesive listed. Installed aluminum filler .020 thick with a .032 thick overlay aluminum doubler to upper side of Aft. lower shell using adhesive and NAS9303B-4 rivets. Applied fiberglass overlays to inner repair as required.

(Bottom side repair at fwd. edge) Cut out area from bottom side of shell assy. as required. Installed a .032 thick aluminum filler with a .040 thick aluminum doubler overlay at the fwd. edge of the shell assy. using adhesive and NAS9303B-4 rivets. Applied fiberglass overlays at both bottom repair areas as required.

(Bottom side repair at boost pump area) Cut out damaged area from shell assy. as required. Replaced stiffener P/N 151-028A50204 using adhesive listed. Installed a .020 thick aluminum filler with a .032 thick aluminum doubler overlay using adhesive and NAS9303B-4 rivets.

Structural repairs made according to the manufacturer's helicopters structural repair manual, except the size of this repair exceeded the limits as specified in the manual.

Approval received for this repair according to data approved on FAA Form 8110-3.

By DER	<u>Louis Bellott</u>	Designation Number	<u>DERT-750020-SW</u>
Dated	<u>11-18-06</u>	8110-3 Serial Number	<u>8110-3 LEB PHI06-27</u>

Weight and balance amended.

**END**

Additional Sheets Are Attached





US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>			
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>		
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>		Address (As shown on registration certificate) Address <b>2001 SE Evangeline Thruway</b>		
			City <b>Lafayette</b>	State <b>LA</b>	
			Zip <b>70508-2156</b>	Country <b>USA</b>	

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address			B. Kind of Agency		
Name	<b>PHI, Inc.</b>			U. S. Certificated Mechanic	Manufacturer
Address	<b>P. O. Box 90808</b>			Foreign Certificated Mechanic	C. Certificate No.
City	<b>Lafayette</b>	State <b>LA</b>	<input checked="" type="checkbox"/>	Certificated Repair Station	<b>HEER617E</b>
Zip	<b>70509-0808</b>	Country <b>USA</b>		Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>Terrell K. Melancon</i> <span style="float: right;"><b>4-1-2012</b></span>
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<b>X</b>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>HEER617E</b>	Signature/Date of Authorized Individual <i>Terrell K. Melancon</i> <span style="float: right;"><b>4-1-2012</b></span>
---	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L  
Nationality and Registration Mark

4-1-2012  
Date

***Structural Repair***

**Repair of Engine Pan at Sta. 166.83**

Part Nomenclature:	<u>Engine Pan Assy.</u>	Part Number:	<u>206-032-315-141</u>
Repair Number:	<u>PHI-206L-RP-0214</u>	Part Serial Number:	<u>N/A</u>
	<u>Rev. IR.</u>		

Work Accomplished: Removed damaged area on LH side of Engine Pan near Sta. 166.83 as required. Fabricated & installed a .012" thick 1/4 hard stainless steel filler with a .020" thick 1/2 hard stainless steel doubler using materials noted in Repair # PHI-206L-RP-0214 Rev. IR.

Structural repair made according to the manufacturer's helicopters structural repair manual, except the size of this repair exceeded the limits as specified in the manual.

Approval received for this repair according to data approved on FAA Form 8110-3.

By DER:	<u>Louis Bellott</u>	Designation Number:	<u>DERT-750020-SW</u>
Dated:	<u>3-28-2012</u>	8110-3 Serial Number:	<u>PHI12-10</u>

Weight and Balance Negligible.

***END***

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Address 2001 SE Evangeline Thruway	
		City Lafayette State LA	Zip 70508-2156 Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic		HEER617E	
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic			
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station			
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>Terrill K. Melancon</i> 4-1-2012
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <i>Terrill K. Melancon</i> 4-1-2012
--	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L

Nationality and Registration Mark

4-1-2012

Date

## ***Structural Repair*** **Repair of Engine Pan at Sta. 172.53**

Part Nomenclature:	<u>Engine Pan Assy.</u>	Part Number:	<u>206-032-315-141</u>
Repair Number	<u>PHI-206L-RP-0071</u>	Part Serial Number	<u>N/A</u>

Removed damaged area on Engine Pan as required. Performed fluorescent penetrant inspection to cut out area. Fabricated & installed .020 ½ hard stainless steel composite bond doubler repair in accordance with Repair # PHI-206L-RP-0071.

Structural repairs made according to the manufacturer's helicopters structural repair manual, except the size of this repair exceeded the limits as specified in the manual.

Approval received for this repair according to data approved on FAA Form 8110-3.

By DER	<u>Michael T. Cox</u>	Designation Number:	<u>DERT-710057-SW</u>
Dated	<u>6-20-03</u>	8110-3 Serial Number:	<u>8110-03311</u>

Weight and Balance Negligible.

**END**

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>		
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>	
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>		Address (As shown on registration certificate) Address <b>2001 SE Evangeline Thruway</b>	
			City <b>Lafayette</b>	State <b>LA</b>
			Zip <b>70508-2156</b>	Country <b>USA</b>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name <b>PHI, Inc.</b>		U. S. Certificated Mechanic		Manufacturer	
Address <b>P. O. Box 90808</b>		Foreign Certificated Mechanic		HEER617E	
City <b>Lafayette</b> State <b>LA</b>		<input checked="" type="checkbox"/> Certificated Repair Station			
Zip <b>70509-0808</b> Country <b>USA</b>		Certificated Maintenance Organization			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>Terrill K. McLaan</i> <span style="float: right;">3-6-2012</span>
--	---

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Flt. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<b>X</b>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>HEER617E</b>	Signature/Date of Authorized Individual <i>Terrill K. McLaan</i> <span style="float: right;">3-6-2012</span>
---	---

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L

Nationality and Registration Mark

3-6-2012

Date

## *Structural Repair*

### **Repair of Roof Beam**

Part Nomenclature: Roof Beam Assy. Part Number: 206-033-210-101  
Repair Number: 12RL-020

Work Accomplished: Blended areas on RH & LH Roof Beam Assembly near Station 148.22 where rollover frames attach. Performed Fluorescent Penetrant Inspection as required. Treated and primed areas, then applied shimming compound to blended areas and sanded smooth as per repair # 12RL-020.

Structural repairs made according to the manufacturer's helicopters structural repair manual, except the size of this repair exceeded the limits as specified in the manual.

Approval received for this repair according to data approved on Form AE-100-1.

Certification Delegate: M. Bouchard Certification Delegate Number: CD 029  
Dated: 3-1-2012 S.O.C. Serial Number: C2586

Weight and Balance Negligible.

**END**

Additional Sheets Are Attached

**BELL HELICOPTER TEXTRON  
CANADA LTD.**

**CIVIL CERTIFICATION**

DAO # 92-Q-01

Cage Code 66738

TRANSPORT CANADA STATEMENT OF COMPLIANCE		DATE: 03/01/12	S.O.C. SERIAL NO: <b>C2586</b>	ISSUE: ---			
		REF: EDNR #	SHEET: 1	OF: 1			
MODEL TYPE: ROTORCRAFT	MODEL (S): 206L-3	CERTIFICATION BASIS (REFERENCE TCDS or GCP): TCDS H-92		REVISION: 24			
CHANGE DEFINITION (Drawing list)			REQUIREMENTS /REFERENCE				
NUMBER	TITLE		Specialty	Cert Para.	MoC		
FIELD REPAIR 12RL-020	STRUCTURAL REPAIR ON ROOF BEAM INSTL P/N 206-033-210-101, ON MODEL 206L-3, S/N 51529, T.T. 16183:05 HOURS, OPERATED BY PHI, INC.  ATTACHMENTS: 11 PAGES DAMAGE DESCRIPTION: 4 PAGES REPAIR INCLUDING COVER MEMO: 7 PAGES  NOTE: THE PURPOSE OF THE DATA IS TO APPROVE THE ENGINEERING ASPECT ONLY OF THE FIELD REPAIRS AND NOT THE WORKMANSHIP AND INSTALLATION.		S	*6.200	A		
			S	*6.201 a), b)	A		
			S	*6.202 a), b)	A		
			S	6.300	R		
			S	6.301	R		
			S	6.302	R		
			S	6.303	R		
			S	6.304a	R		
			S	6.306	R		
			Paragraphs not listed are considered to have negligible or no effect with respect to the listed changes				
			LIST OF SUBSTANTIATING DOCUMENTATION				
			NUMBER	TITLE	TCCA LOI		
*	Electronic stress notes filed in: T:\PSE\206\STRESS NOTE\REPAIR\09RL-055B.xlsm	N/A					
Paragraphs not listed are considered to have negligible or no effect with respect to the listed changes							
CERTIFICATION			NA=TCCA LOI Not Applicable NIL=No LOI Identified by TCCA FI=For Info WI=Witness or Inspection RA=Review & Acceptance	R=Review A=Analysis I=Inspect FA=Failure Analysis N=Noted	TC=Test Ground TF=Test Flight		
I THEREFORE RECOMMEND APPROVAL OF THESE DATA	03/02/2012						
I THEREFORE APPROVE THESE DATA							
CERTIFICATION DELEGATE	M. BOUCHARD						
CERTIFICATION DELEGATE No	CD 029						
LOA SPECIALTY	S						
LoA SPECIALTY Legend: FTP = Flight Test/Pilot; FT/E = Flight Test/Engineer; OSES = Occupant Safety and Environmental Systems; FHMCS = Fuel and Hydromechanical Control Systems; S = Structures; PE = Powerplants and Emissions; AE = Avionics and Electrical Systems; EEDA = Electronic Equipment Design Assurance							

12RL-020

S/N 51529

PHI, INC.

MODEL 206L-3

ROOF BEAM INSTL

PART NUMBER 206-033-210-101

**NOTES:** Refer to BHT-ALL-SRM, Sections 3 & 4 and to Appendix "A" respectively for process sheets and for standard repair materials.

Refer to BHT-ALL-SPM, Section 13 for Item C-xxx definitions.

All repair procedures to be carried out in accordance with BHT-ALL-SRM approved processes.

The Instructions for Continued Airworthiness (ICA) are not affected by this repair.

**1.0 REQUIRED:**

- |     |                            |  |
|-----|----------------------------|--|
| 1.1 | Chemical film material:    | (Item C-100)   |
| 1.2 | Epoxy polyamide primer:    | (Item C-204)   |
| 1.3 | Shimming compound:         | (Item C-567)   |
|     | Mix ratio:                 | 100 parts A for 14.4 parts B by weight   |
|     | Pot life at 65° to 85°F:   | 1 hour   |
|     | Drill time at 65° to 85°F: | A minimum hardness of Shore D 55<br>(Recommended minimum 4 hours)  |
|     | Cure time at 65° to 85°F:  | A minimum hardness of Shore D 75<br>(Recommended minimum 6 hours)  |
| 1.4 | Cleaners:                  | Methyl-Ethyl-Ketone (MEK) (Item C-309)<br>Acetone (Item C-316)<br>Isopropyl Alcohol (Item C-385)<br>Aliphatic Naphtha (Item C-305)<br>Toluene (Item C-306) |
| 1.5 | Process Sheets:            | Chemical Film Application on Aluminum and Titanium Parts (Para. 3-2-14)  |



12RL-020

S/N 51529

**2.0 PROCEDURE:**

2.1 Gain access to damaged areas recording type, size and location of fasteners removed. See figure 1 & detail A.

2.2 **Notes:** Do not increase existing depth of damage.  
Ensure smooth transition all around reworked areas.  
Surface roughness of roof beams upper outboard flange shall be 63 microinches or better.  
Suggested method: lightly abrade roof beams upper outboard flange surface with 180 grit or finer abrasive paper.  
Blending to be performed by sanding in beams longitudinal (FWD-AFT) direction.

Blend damaged area of LHS/RHS roof beams upper outboard flange smooth as shown in view B-B and section C-C. Maintain a 20:1 length to depth ratio along roof beam axis. Minimum remaining material thickness to be 0.095 inch for LHS beam and 0.090 inch for RHS beam as shown in detail D.

2.3 **Note:** Care to be taken to prevent fluorescent penetrant fluid from flowing between roof beams and roof shell.

Fluorescent penetrant inspect reworked area of LHS/RHS roof beams to ensure no damage remains and surface is smooth per BHT-ALL-SPM, paragraphs 6-2 to 6-16. If such damage exists, resubmit to PSE with additional information describing extent of damage.

2.4 **Note:** Do not apply excessive amount of chemical film material.

Brush apply chemical film material specified in 1.1 to reworked area of LHS/RHS roof beams upper outboard flange.

2.5 Prime all bare metal surfaces using material specified in 1.2. Allow to dry.

2.6 Lightly abrade primer to remove gloss.

2.7 **Note:** Do not soak parts to be bonded with cleaner specified in 1.4. Use of a moistened rag is recommended.

Clean reworked areas.

2.8 **Note:** There shall be no substitute for shimming compound specified in 1.3. Refer to step 1.3 for shimming compound preparation and curing data.

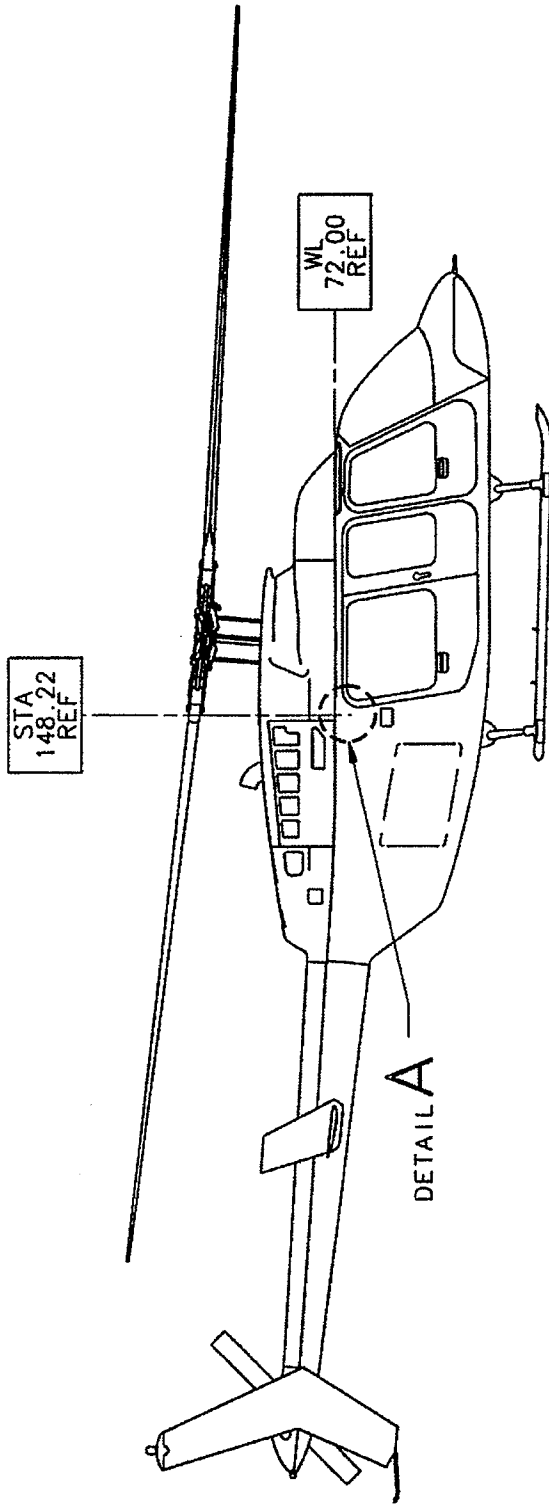
Prepare shimming compound specified in 1.3.

12RL-020

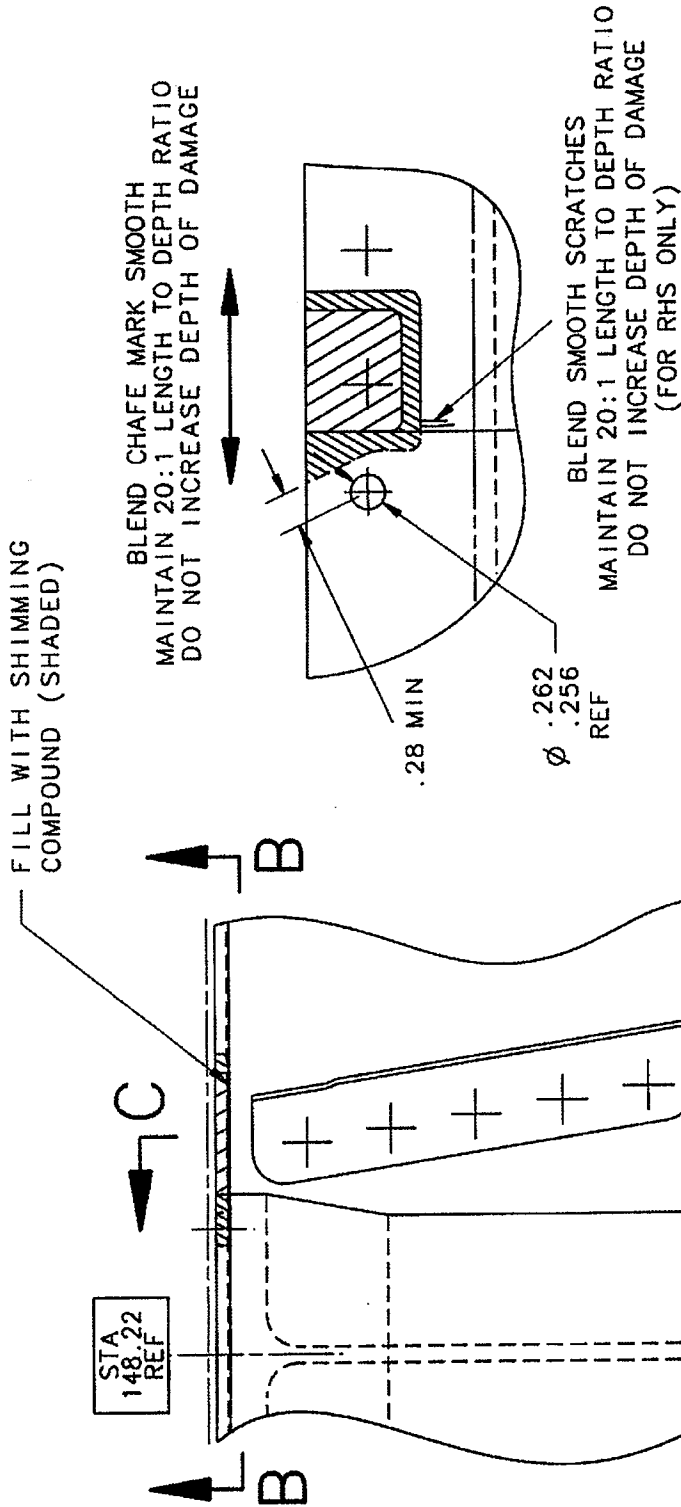
S/N 51529

2.0 PROCEDURE: (CONT'D)

- 2.9 Apply shimming compound specified in 1.3 to faying surface of reworked areas of LHS/RHS roof beams. See detail A.
- 2.10 Allow to cure at room temperature for 6 hours.
- 2.11 Sand shimming compound smooth to surface of LHS/RHS roof beams upper outboard flange.
- 2.12 **Note: Do not soak parts to be bonded with cleaner specified in 1.4. Use of a moistened rag is recommended.**  
  
Clean reworked areas.
- 2.13 Inspect cured shimming compound for discrepancies. Pinholes, porosity and voids less than 0.15 inch diameter and at least one inch apart are acceptable as is.
- 2.14 If required, touch-up primer. Allow to dry.
- 2.15 Ensure means are taken to prevent further chafing.
- 2.16 Re-finish as required.



TITLE: 206L-3 REPAIR ROOF BEAM INSTL	PREPARED BY: S. LORTIE	REF P/N: 206-033-210-101	SHEET: 1 OF 3
DAMAGED P/N: 206-033-213-003, RHS BEAM 206-033-213-001, LHS BEAM	UNITS: INCHES	DATE: 12/03/01	DWG: 206-033-210-12RL-020





VIEW B-B

NOTE: SOME PARTS NOT SHOWN FOR CLARITY

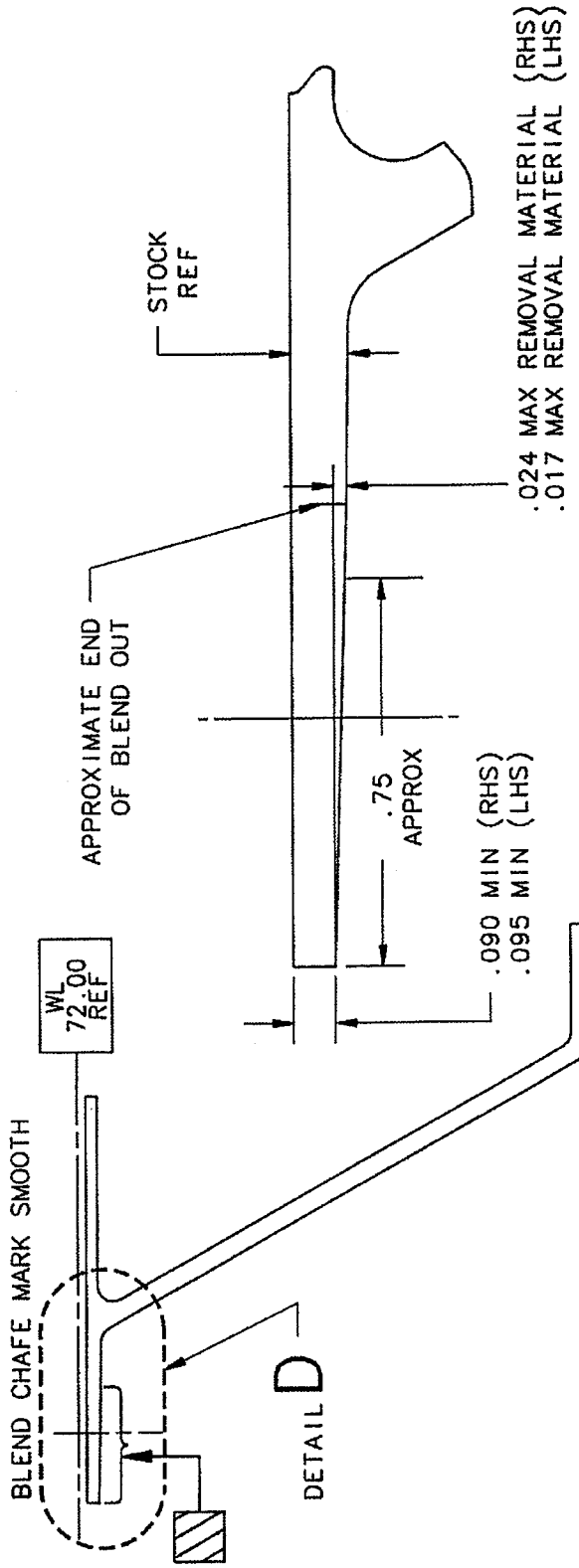
DETAIL A

TYPICAL FOR LHS EXCEPT AS SHOWN

-  DAMAGED AREA
-  APPROXIMATE OUTLINE OF REWORKED AREA

FASTENERS  
 + EXISTING FASTENER

TITLE: 206L-3 REPAIR ROOF BEAM INSTL	PREPARED BY: S. LORTIE	REF P/N: 206-033-210-101	SHEET: 2 OF 3
DAMAGED P/N: 206-033-213-003, RHS BEAM 206-033-213-001, LHS BEAM	UNITS: INCHES	DATE: 12/03/01	DWG: 206-033-210-12RL-020



### DETAIL D

NOTE: SHIMMING COMPOUND NOT SHOWN FOR CLARITY PURPOSE

### SECTION C-C

NOTE: SHIMMING COMPOUND NOT SHOWN FOR CLARITY PURPOSE

TYPICAL FOR LHS EXCEPT AS SHOWN

▨ APPROXIMATE OUTLINE OF REWORKED AREA


TITLE: 206L-3 REPAIR ROOF BEAM INSTL	PREPARED BY: S. LORTIE	REF P/N: 206-033-210-101	SHEET: 3 OF 3
DAMAGED P/N: 206-033-213-003, RHS BEAM 206-033-213-001, LHS BEAM	UNITS: INCHES	DATE: 12/03/01	DWG: 206-033-210-12RL-020

**Hurst, Ken**

**From:** Hurst, Ken  
**Sent:** Friday, February 24, 2012 9:01 AM  
**To:** 'pselight'  
**Subject:** 206L-3 S/N 51529 DAMAGE (CHAFE) ROOF BEAM P/N 206-033-210-125 (REF PHI-206L-DA-0211) REQUESTING APPROVED REPAIR PROCEDURE

**Attachments:** PHI-206L-DA-0211.pdf; image001.jpg

**Structural Repair Request Form**

Date: 2/24/2012	Number of pages (including this one): 2
Send to:  (817) 280-2635 [for model 214 (all variants)]	(450) 433-0272 [for all other commercial models]
Sender: <u>Ken Hurst</u>	Tel: <u>(337) 272-4593</u>
Company: <u>PHI, Inc.</u>	Fax: <u>(337) 272-4575</u>
Email address: <u>khurst@phihelico.com</u>	<input checked="" type="checkbox"/> Owner and/or Operator Repair Facility

Dear PSE, please provide us with an approved repair for the damage described below.  
 Thanks, KH (your initials)

Aircraft Information	
Model: 206L3	Serial Number: 51529
Flight Time: 16,183:05	Registration: N3116L
Status: Routine	<input checked="" type="checkbox"/> Work Stoppage AOG
Damage Description	
Part Number: 206-033-210-125	Serial Number:
Tailboom Part Number (if applicable):	
Tailboom Serial Number (if applicable):	
Description: (attach a sketch) The following attachment (PHI-206L-DA-0211) shows damage (chafe) on right hand side of roof beam in area of roll over bulkhead.  I will be sending you chafe damage on the left and side also.  There is work stoppage in this area.  Thank you, Ken Hurst PHI, Inc. Engineering Department	

This form may also be scanned and email with the sketch and/or other attachments to the appropriate electronic address below for action.


[pselight@bellhelicopter.textron.com](mailto:pselight@bellhelicopter.textron.com) for 47, 206, and 407 (all variants)  
[psemedium@bellhelicopter.textron.com](mailto:psemedium@bellhelicopter.textron.com) for 204, 205, 212, 412 (all variants)  
[pseinter@bellhelicopter.textron.com](mailto:pseinter@bellhelicopter.textron.com) for 222, 230, 427, 430 (all variants)  
[psemil214@bellhelicopter.textron.com](mailto:psemil214@bellhelicopter.textron.com) for 214 (all variants)

Ref: IL GEN-04-96



FWD →

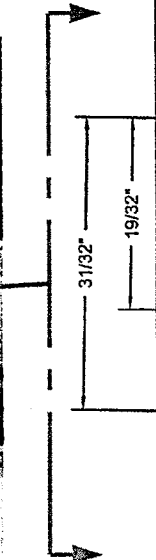
VIEW LOOKING AT DAMAGE AREA  
(CHAFE) ON RHS OF BEAM

 <b>PHI, INC.</b> LAFAYETTE, LOUISIANA	DWG NO: PHI-206L-DA-0211	FILE NO: 0062980A.DWG	REG NO: N3116L
	TITLE: DAMAGE AREA (CHAFE) ON RHS OF BEAM P/N: 206-033-210-125, ACFT S/N: 51529		S/N: 51529
PREP BY: S. TRAHAN		REV/DATE: IR 2/23/12	MODEL: 206L-3
			T/I: 16, 183: 05
			SHEET 1 OF 1

BX11RP\_1



.114"  
BEAM  
THICKNESS




FWD →

**Hurst, Ken**

**From:** Hurst, Ken  
**Sent:** Friday, February 24, 2012 9:14 AM  
**To:** 'pselight'  
**Subject:** 206L-3 S/N 51529 DAMAGE (CHAFE) LHS OF ROOF BEAM P/N 206-033-210-125 (REF PHI-206L-DA-0212) REQUESTING APPROVED REPAIR PROCEDURE  
**Attachments:** PHI-206L-DA-0212.pdf; image001.jpg

**Structural Repair Request Form**

Date: 2/24/2012	Number of pages (including this one): 2
Send to:  <b>Bell Helicopter</b> A Textron Company	(817) 280-2635 [for model 214 (all variants)] (450) 433-0272 [for all other commercial models]
Sender: <u>Ken Hurst</u>	Tel: <u>(337) 272-4593</u>
Company: <u>PHI, Inc.</u>	Fax: <u>(337) 272-4575</u>
Email address: <u>khurst@phihelico.com</u>	<input checked="" type="checkbox"/> Owner and/or Operator Repair Facility

Dear PSE, please provide us with an approved repair for the damage described below.  
 Thanks, KH (your initials)

Aircraft Information	
Model: 206L3	Serial Number: 51529
Flight Time: 16,183:05	Registration: N3116L
Status: <input type="checkbox"/> Routine	<input checked="" type="checkbox"/> Work Stoppage <input type="checkbox"/> AOG
Damage Description	
Part Number: 206-033-210-125	Serial Number:
Tailboom Part Number (if applicable):	
Tailboom Serial Number (if applicable):	
Description: (attach a sketch) The following attachment (PHI-206L-DA-0212) shows damage (chafe) on left hand side of roof beam in area of roll over bulkhead.  There is work stoppage in this area.	
Thank you, Ken Hurst PHI, Inc. Engineering Department	

This form may also be scanned and email with the sketch and/or other attachments to the appropriate electronic address below for action.

- [pselight@bellhelicopter.textron.com](mailto:pselight@bellhelicopter.textron.com) for 47, 206, and 407 (all variants)
- [psemedium@bellhelicopter.textron.com](mailto:psemedium@bellhelicopter.textron.com) for 204, 205, 212, 412 (all variants)
- [pseinter@bellhelicopter.textron.com](mailto:pseinter@bellhelicopter.textron.com) for 222, 230, 427, 430 (all variants)
- [psemil214@bellhelicopter.textron.com](mailto:psemil214@bellhelicopter.textron.com) for 214 (all variants)

Ref: IL GEN-04-96







US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

**INSTRUCTIONS:** Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) Address 2001 SE Evangeline Thruway	
		City Lafayette State LA	Zip 70508-2156 Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <u>PHI, Inc.</u>		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address <u>P. O. Box 90808</u>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City <u>Lafayette</u> State <u>LA</u>		<input checked="" type="checkbox"/> Certificated Repair Station	HEER617E
Zip <u>70509-0808</u> Country <u>USA</u>		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>William D. Smith 10-1-07</i>
--	--

### 7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Flt. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <i>William D. Smith 10-1-07</i>
--	--

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L

Nationality and Registration Mark

10-1-07

Date

**STC Installation**

Installed replacement battery door in accordance with Aeronautical Accessories, Inc. STC SR00044AT installation report number AA-91057, Revision M, dated December 6, 2001.

Weight change is negligible.

END

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N3116L</u>	Serial No. <u>51529</u>	
	Make <u>Bell</u>	Model <u>206</u>	Series <u>L-3</u>
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHI, Inc.</u>	Address (As shown on registration certificate) <u>Quality Assurance 2001 SE Evangeline Thruway</u>	
		City <u>Lafayette</u>	State <u>LA</u>
		Zip <u>70508-2156</u>	Country <u>USA</u>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	<u>BELL HELICOPTER TEXTRON, INC</u>	<input type="checkbox"/> U.S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address	<u>301 W. PONT DES MOUTON ROAD</u>	<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City	<u>LAFAYETTE</u> State <u>LA</u>	<input checked="" type="checkbox"/> Certified Repair Station	<u>CRS B1H2300N Limited Airframe</u>
Zip	<u>70507</u> Country <u>U.S.A.</u>	<input type="checkbox"/> Certified Maintenance Organization	<u>Limited Specialized Services</u>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>3/2/12</u>
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### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Inspection Authorization	Other (Specify)	
Certificate or Designation No. <b>B1H2300N</b>		Signature/Date of Authorized Individual <u>JASON PICARD</u> <u>3/2/12</u>			

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: RH SIDE PANEL**  
**ORIGINAL PART NUMBER: 206-033-115-183**  
**SERIAL NUMBER: BP0000138**  
**WORK ORDER NUMBER: W184322AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-055 DATED  
9/15/99.**

-----END-----

Additional Sheets are Attached



U.S. Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR 43 Sec.43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. Sec. 44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. Sec. 46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <u>N233PH</u>	Serial No. <u>51529</u>
	Make <u>Bell</u>	Model <u>206</u> Series <u>L-3+</u>
<b>2. Owner</b>	Name (As shown on registration certificate) <u>PHE INC</u>	Address (As shown on registration certificate) <u>2001 SE Evangeline Thruway</u>
		City <u>Lafayette</u> State <u>LA</u> Zip <u>70508-2156</u> Country <u>USA</u>

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	~~~~~	(As described in Item 1 above)	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	POWERPLANT	Rolls Royce	250 L-30P	LAE-895111 LAD-95098
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

<b>A. Agency's Name and Address</b>		<b>B. Kind of Agency</b>	
Name	<u>H-S Tool &amp; Parts Inc.</u>	<input type="checkbox"/>	U.S. Certificated Mechanic
Address	<u>#140-2560 Simpson Road</u>	<input checked="" type="checkbox"/>	Foreign Certificated Mechanic
City	<u>Richmond</u> State <u>BC</u>	<input type="checkbox"/>	Certificated Repair Station
Zip	<u>V6X 2P9</u> Country <u>Canada</u>	<input type="checkbox"/>	Certificated Maintenance Organization
		<b>C. Certificate No.</b> <u>39-87</u>	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B	<input type="checkbox"/>	Signature/Date of Authorized Individual <u>C. Trsek</u>	19/Dec/2011
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**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED     REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	<input checked="" type="checkbox"/>	Persons Approved by Canadian Department of Transport
	FAA Designee	Repair Station	Inspection Authorization		Other (Specify)

Certificate or Designation No. <u>39-87</u>	Signature/Date of Authorized Individual <u>C. Trsek</u>	19/Dec/2011
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N233PH  
Nationality and Registration Mark

3/10/13  
Date

Description: Rolls Royce 250 Gearbox Housing  
P/N 23035139, S/N HL39646

1. Repaired (Qty 3) Bolt Holes On Splitline IAW 14W3 72-00-00.
2. Repaired Engine Mounts IAW RSS 1012 And E.O. # 128.
3. Repaired Oil Pump Drive Seal Bore And Flange IAW RSS 1051.
4. Replaced (Qty 4) Oil Pump Drive Studs IAW 14W3 72-00-00.
5. Repaired PT Tachometer Governor Locating Bore And Flange IAW RSS 1051.
6. Replaced (Qty 4) PT Tachometer Governor Studs IAW 14W3 72-00-00.
7. Final Inspection.

Repair Salvage Schemes (RSS) 1012 And 1051 Are OEM Approved Data.

All Records of Work Performed Are On File Under Work Order 66431.

No Further Statements to Follow.



Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No.  
2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

**INSTRUCTIONS:** Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N3116L</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L-3</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI, Inc.</b>	Address (As shown on registration certificate) <b>Quality Assurance 2001 SE Evangeline Thruway</b>	
		City <b>Lafayette</b>	State <b>LA</b>
		Zip <b>70508-2156</b>	Country <b>USA</b>

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
<input type="checkbox"/>	<input type="checkbox"/>		Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	<b>BELL HELICOPTER TEXTRON, INC.</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address	<b>301 W. PONT DES MOUTON ROAD</b>	<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City	<b>LAFAYETTE</b> State <b>LA</b>	<input checked="" type="checkbox"/> Certified Repair Station	<b>CRS B1H2300N Limited Airframe</b>
Zip	<b>70507</b> Country <b>U.S.A.</b>	<input type="checkbox"/> Certified Maintenance Organization	<b>Limited Specialized Services</b>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual  <b>3/2/12</b>
--	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Flt. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Inspection Authorization	Other (Specify)
Certificate or Designation No. <b>B1H2300N</b>		Signature/Date of Authorized Individual <b>JASON PICARD</b>  <b>3/2/12</b>		



## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3116L	05/09/2012
Nationality and Registration Mark	Date

**NOMENCLATURE: ROOF**

**ORIGINAL PART NUMBER: 206-033-201-267**

**UPGRADED TO PART NUMBER: 206-033-201-333**

**SERIAL NUMBER: BP0000135**

**WORK ORDER NUMBER: W183139AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH BELL HELICOPTER  
TEXTRON, INC. FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-049,  
DATED 4/19/99 AND UPGRADED IN ACCORDANCE WITH OVERHAUL PROCEDURE PHI-450-  
OP-050 DATED 10/15/02, REVISION E.**

-----**END**-----

Additional Sheets are Attached



US Department of Transportation

Federal Aviation Administration

# MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved

OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each violation (Section 901 of Federal Aviation Act of 1958).

1. Aircraft	Make Bell	Model 206L-3
	Serial No. 51529	Nationality and Registration Mark N3116L
2. Owner	Name (As shown on registration certificate) PHI, Inc.	Address (As shown on registration certificate) 2001 SE Evangeline Thruway Lafayette, LA 70508-2156

### 3. For FAA Use Only

### 4. Unit Identification

### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

A. Agency's Name and Address  <b>ACADIAN COMPOSITES</b> 301 W. PONT DES MOUTON ROAD LAFAYETTE, LA 70507	B. Kind of Agency	C. Certificate No.
	<input type="checkbox"/> U.S. Certificated Mechanic	CRS BIH2300N
	<input type="checkbox"/> Foreign Certificated Mechanic	LIMITED AIRFRAME
	<input checked="" type="checkbox"/> Certified Repair Station	LIMITED SPECIALIZED SERVICES
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date 6-24-05	Signature of Authorized Individual 
-----------------	--

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	X Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection 6-24-05	Certificate or Designation No. B1H2300N	Signature of Authorized Individual JIMMIE MELANCON		

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**NOMENCLATURE: SEAT PANEL  
ORIGINAL PART NUMBER: 206-033-172-117  
SERIAL NUMBER:  
WORK ORDER NUMBER: W59573AA**

**OVERHAULED PANEL MENTIONED ABOVE IN ACCORDANCE WITH ACADIAN COMPOSITES  
FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-008, REVISION A, DATED  
8/3/99.**

-----**END**-----

Additional Sheets are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**  
Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <i>Bell</i>	Model <i>206L-3</i>
	Serial No. <i>51529</i>	Nationality and Registration Mark <i>N3116L</i>
<b>2. Owner</b>	Name (As shown on registration certificate) <i>PHT, INC</i>	Address (As shown on registration certificate) <i>2001 SE Evangeline Thruway Lafayette, La. 70508-2156</i>

**3. For FAA Use Only**

4. Unit Identification				5. Type		
Unit	Make	Model	Serial No.	Repair	Alteration	
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~					
POWERPLANT	<i>Rolls Royce</i>	<i>250/C-30P</i>	<i>CAE-895845 CAC-90940</i>	<b>X</b>		
PROPELLER						
APPLIANCE	Type					
	Manufacturer					

**6. Conformity Statement**

<b>A. Agency's Name and Address</b> CADORATH AEROSPACE INC. 2070 LOGAN AVENUE WINNIPEG, MANITOBA R2R 0H9	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certified Mechanic <input checked="" type="checkbox"/> Foreign Certified Mechanic <input type="checkbox"/> Certified Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b>  <div style="text-align: center; font-size: 1.2em;"><b>86-91</b></div>
---	--	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <i>FEB 16 2006</i>	Signature of Authorized Individual <i>R.J. George</i> <span style="float: right; border: 1px solid black; border-radius: 50%; padding: 2px;">CAI 08</span>
----------------------------	---

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <i>FEB 16 2006</i>		Certificate or Designation No. <i>86-91</i>	Signature of Authorized Individual <i>R.J. George</i> <span style="float: right; border: 1px solid black; border-radius: 50%; padding: 2px;">CAI 08</span>	

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

REF. W/O #                      SHROUD HOUSING                      P/N# 23056637                      S/N#      MK17508  
41538                                      250-C30

TSO:      0                                      TSN:      UNK                                      CYCLES:      UNK

- ITEM
- 1 O/H INSPECTED IAW 250 C30 14W3 O/H/M 2ND EDITION REV.11
  - 2 NDT IAW PRPL 1-8
  - 3 SERMETEL PAINTED IAW ROLLS ROYCE PRPL 1-7
  - 4 ALSEAL COATED CONTOUR IAW 14W3 ED.2 REV.11
  - 5 FINAL INSPECTION

**ADDITIONAL MAINTENANCE REQUIRED:**  
NONE

**ALL RECORDS OF WORK PERFORMED ARE ON FILE UNDER W/O:**                      41538  
**NO FURTHER STATEMENTS TO FOLLOW.**

FEB 16 2006

DATE: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_



K. J. GEORGE

STAMP



Additional Sheets Are Attached

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

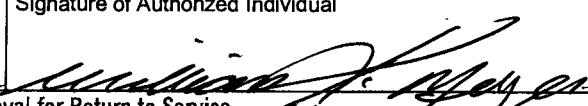
5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address <b>Petroleum Helicopters, Inc. P.O Box 90808 Lafayette, LA. 70509</b>	B. Kind of Agency <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <b>HEER617E Airframe, Powerplant &amp; Accessories Ltd. Radio Class 1,2,3</b>
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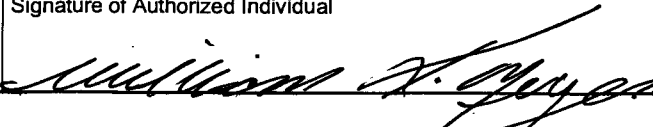
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-13-2001</b>	Signature of Authorized Individual 
---------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-13-2001</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual 
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**NOTICE**

Weight and balance or operating limit changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

**Installed Aircraft Parts Corporation, starter generator in accordance with STC SH123NE.**

----- **END** -----

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

**For FAA Use Only**  
Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <b>Bell</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>Fleet Credit Corporation</b>	Address (As shown on registration certificate) <b>111 Westminster Street Providence, RI 02903</b>

### 3. For FAA Use Only

### 4. Unit Identification

### 5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>19 DEC. 1991</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
-----------------------------	---

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <b>19 DEC. 1991</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>James R. Edwards</i> <b>EDWARDS</b>
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed starter sequence according to Petroleum Helicopters, Inc. Drawing Number 206-L-RI-5114 each sheets, Drawing Number PHI 11647 2 each sheets, Drawing Number 10140 and Drawing Number 11200 sheet 2a of 2.

Checked switch installation to verify that it would not cause throttle to roll off the stop.

This installation approved for VFR flights only.

This modification is in accordance with data previously approved on FAA form dated October 21, 1986 for Bell Model 206L-3 N31821 S/N 51076 and approved for duplication on identical aircraft make, model and altered configuration when accomplished by the original modifier.

-----END-----

Additional Sheets Are Attached

US Department of Transportation  
Federal Aviation Administration

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopters</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue of the Americas New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	(As described in item 1 above)				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
<b>Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA. 70509</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>HEER617E Airframe, Powerplant &amp; Accessories Ltd. Radio Class 1,2,3</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>10-14-2001</b>	Signature of Authorized Individual <i>William A. Keyser</i>
---------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>10-14-2001</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <i>William A. Keyser</i>
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## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### STC INSTALLATIONS

1. Installed Aeronautical Accessories, Inc. folding maintenance step assembly according to STC-SH2243SO.
2. Installed Aeronautical Accessories, Inc. step handles in accordance with STC-SH1366SO.
3. Installed Aeronautical Accessories, Inc. air deflectors on crew and passenger door windows in accordance with STC-SR01582AT.
4. Installed Altair Avionics Corporation-Allied Signal Aerospace Intellistart plus Electronic Control System in accordance with STC-SR00113BO, and PHI Drawing Number PHI-206-60387 rev. IR . This installation is in conjunction with Altair Avionics Corporation-Allied Signal Aerospace Intellistart Electronic control Unit in accordance with STC -SE00026EN for the engine.
5. Installed rubber mounted chin windows in accordance with Aeronautical Accessories STC-SH5850SW.

----- E N D -----

Additional Sheets Are Attached



US Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

**INSTRUCTIONS:** Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

<b>1. Aircraft</b>	Make <p style="text-align: center;">Bell</p>	Model <p style="text-align: center;">206L-3</p>
	Serial No. <p style="text-align: center;">51529</p>	Nationality and Registration Mark <p style="text-align: center;">N3116L</p>
<b>2. Owner</b>	Name (As shown on registration certificate) <p style="text-align: center;">Fleet Credit Corporation</p>	Address (As shown on registration certificate) <p style="text-align: center;">111 Westminster Street Providence, RI 02903</p>

### 3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

### 6. Conformity Statement

<b>A. Agency's Name and Address</b> Petroleum Helicopters, Inc. P.O. Box 90808 Lafayette, LA 70509	<b>B. Kind of Agency</b> <input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	<b>C. Certificate No.</b> AIRFRAME, POWERPLANT & ACCESSORIES LIMITED RADIO CLASS 1, 2, & 3 CRS NO. HEER617E
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <p style="text-align: center;">19 DEC. 1991</p>	Signature of Authorized Individual <p style="text-align: center;"><i>James R. Edwards</i></p> <p style="text-align: right;"><b>EDWARDS</b></p>
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### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

<b>BY</b>	FAA Flt. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection <p style="text-align: center;">19 DEC. 1991</p>	Certificate or Designation No. <p style="text-align: center;">HEER617E</p>	Signature of Authorized Individual <p style="text-align: center;"><i>James R. Edwards</i></p> <p style="text-align: right;"><b>EDWARDS</b></p>
--	---	---

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

#### TAILBOOM MOUNTED ANTENNA

Installed whip antenna at station 240 on lower tailboom skin antenna mount base reinforced with 2024T3 .040 aluminum using MS20470AD-4 rivets. Reinforcing plates are protected from corrosion and do not interfere with required inspections. See PH1 Drawing Number 206L-3-RI-513.

Wiring was installed in a suitable manner for protection against fuel, oil, water, and other detrimental substances and abrasion damage, also supported by insulated clamps to prevent chaffing according to AC 43.13-1A and AC 43.13-2A.

Weight and balance computed.

No compass correction is necessary.

-----END-----

# NOT INSTALLED

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

OMB No. 2120-0020 Electronic Tracking Number  
Exp: 5/31/2018

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark <b>N233PH</b>	Serial No. <b>51529</b>	
	Make <b>Bell</b>	Model <b>206</b>	Series <b>L3+</b>
<b>2. Owner</b>	Name (As shown on registration certificate) <b>PHI INC</b>	Address (As shown on registration certificate) <b>2001 SE Evangeline Thruway</b>	
		City <b>Lafayette</b>	State <b>LA</b>
		Zip <b>70508-2156</b>	Country <b>USA</b>

3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

<b>A. Agency's Name and Address</b> Name: <b>Paul Parker</b> Address: <b>7807 Juniper Creek Dr. East</b> City: <b>Wilmer</b> State: <b>AL</b> Zip: <b>36587</b> Country: <b>USA</b>	<b>6. Conformity Statement</b> B. Kind of Agency <input checked="" type="checkbox"/> U. S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Certificated Maintenance Organization C. Certificate No. <b>AP 3087500</b>
---	---

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <b>Paul Parker 9-6-15</b>
--	--

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)

Certificate or Designation No. <b>AP 425310022 IA</b>	Signature/Date of Authorized Individual <b>John R. Payne AP 425310022 IA 9-6-15</b>
--	--

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N233PH  
Nationality and Registration Mark

9-6-15  
Date

Installed Van Horn Tail Rotor Blades in accordance with Van Horn Aviation L.L.C. STC Number SR02249LA Master Document List (MDL) 206 MDL-001, Revision G, 07-19-2011 and Instructions for continued Airworthiness ICA Manual NO. VMM-206L3-307, Revision H 10-09-2013.

FAA approved Rotorcraft Flight Manual Supplement, Document NO 206L4-FMS-904 Revision 2, 07-02-2012 is a part of this installation and is installed in the Rotorcraft Flight Manual.

Weight and Balance Negligible

Additional Sheets Are Attached

US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION  
(Airframe, Powerplant, Propeller, or Appliance)**

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This form is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>Bell Helicopter</b>	Model <b>206L-3</b>
	Serial No. <b>51529</b>	Nationality and Registration Mark <b>N3116L</b>
2. Owner	Name (As shown on registration certificate) <b>CIT Leasing Corporation</b>	Address (As shown on registration certificate) <b>1211 Avenue Of The Americas FL 21 New York, NY 10036-8701</b>

3. For FAA Use Only

4. Unit Identification

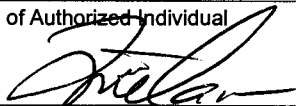
5. Type

Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>~~~~~ (As described in item 1 above) ~~~~~</i>				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address <b>Acadian Composites, L.L.C. 301 West Pont Des Mouton Rd. Lafayette, La. 70507</b>	B. Kind of Agency U.S. Certificated Mechanic Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station Manufacturer	C. Certificate No. <b>CRS NO. HEER617E Airframe, Powerplant, Accessories Limited Class 1,2, and 3</b>
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D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>3-28-02</b>	Signature of Authorized Individual 
------------------------	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  APPROVED  REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Person Approved by Transport Canadian Airworthiness Group	

Date of Approval or Rejection <b>3-28-02</b>	Certificate or Designation No. <b>HEER617E</b>	Signature of Authorized Individual <b>Jimmie Melancon</b> 
---	---	--



**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

Nomeclature: Vertical Fin  
Original Part Number: 206-020-113-255  
Serial Number: BP3092  
Work Order Number: J0221506

OVERHAULED ABOVE MENTIONED PANEL IN ACCORDANCE WITH PETROLEUM HELICOPTERS, INC. D.B.A. ACADIAN COMPOSITES, L.L.C., FAA APPROVED OVERHAUL PROCEDURE NUMBER PHI-450-OP-041, REVISION A, DATED 11/23/99.

----- E N D -----

**NOT  
INSTALLED**

Additional Sheets Are Attached



US Department of Transportation  
Federal Aviation Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

<b>1. Aircraft</b>	Nationality and Registration Mark N3116L	Serial No. 51529	
	Make Bell	Model 206	Series L-3
<b>2. Owner</b>	Name (As shown on registration certificate) PHI, Inc.		Address (As shown on registration certificate) Quality Assurance 2001 SE Evangeline Thruway
			City: Lafayette State: LA Zip: 70508-2156 Country: USA

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Name: PHI, Inc.		<input type="checkbox"/> U. S. Certificated Mechanic		HEER617E	
Address: P. O. Box 90808		<input type="checkbox"/> Foreign Certificated Mechanic			
City: Lafayette State: LA		<input checked="" type="checkbox"/> Certificated Repair Station			
Zip: 70509-0808 Country: USA		<input type="checkbox"/> Certificated Maintenance Organization			

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>THOMAS LANE Thomas Lane</i> 5-21-2012
--	---

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is  Approved  Rejected

BY	FAA Fit. Standards Inspector		Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	X	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. HEER617E	Signature/Date of Authorized Individual <i>THOMAS LANE Thomas Lane</i> 5-21-2012
--	---

**NOTICE**

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

**8. Description of Work Accomplished**

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N3116L

Nationality and Registration Mark

5-21-2012

Date

**STC INSTALLATIONS**

Installed windshields in accordance with Aeronautical Accessories, Inc. STC SH1664SO and Installation Instructions Report AA-89001, Revision I, dated April 17, 2000.

Installed skylights in accordance with Aeronautical Accessories, Inc. STC SH1664SO and Installation Instructions Report AA-90021, Revision E, dated April 2, 1998.

Actual weight and balance complied with.

**END**

Additional Sheets Are Attached

United States of America  
Department of Transportation — Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SH1664SO

*This certificate, issued to* Aeronautical Accessories, Inc.  
P.O. Box 3689  
Bristol, Tennessee 37625

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 6 of the Civil Air Regulations.*

*Original Product — Type Certificate Number:* H2SW  
*Make:* Bell  
*Model:* 206A, 206B, 206L, 206L-1, 206L-3, 206L-4

*Description of Type Design Change:* Installation of replacement windows and windshields in accordance with Aeronautical Accessories, Inc. Report No. KR-362, Drawing List Replacement Windshield and Window Installation dated August 23, 1984, or later FAA approved revision.

*Limitations and Conditions:* This approval should not be extended to other rotorcraft of this model on which other previously approved modifications are incorporated unless it is determined by the installer that the interrelationship between this change and any of those other previously approved modifications will produce no adverse effect upon the airworthiness of that rotorcraft. This determination should include consideration of significant changes in weight distribution such as an increase in the fixed disposable weight of the fuselage.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked, or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* May 9, 1984

*Date reissued:*

*Date of issuance:* August 29, 1984

*Date amended:* October 5, 1992



*By direction of the Administrator*  
*Paul C. Scorsone*  
Paul C. Scorsone  
Acting Manager, (Atlanta Aircraft  
Certification Office

(Title)

*Any alteration of this certificate is punishable by a fine of not exceeding \$1,000, or imprisonment not exceeding 3 years, or both.*

*This certificate may be transferred in accordance with FAR 21.47.*



**Aeronautical Accessories, Inc**  
**P.O. BOX 3689**  
**BRISTOL, TENNESSEE 37625-3689 U.S.A.**  
**TELEPHONE: 423-538-5151 800-251-7094**  
**TELEFAX: 423-538-8469 E-MAIL: sales@aero-access.com**

**TO: Petroleum Helicopters, Inc.**  
**ATTN: Lee Prejean**  
**DATE: January 31, 2007**  
**REF: Authorization Letter**

**Aeronautical Accessories, Inc. authorizes your company to install all of our "STC" products on your "fleet of aircraft".**

**Our "STC" products are listed in our product catalog.**

**Regards,**  
**James Rumley**  
**Marketing/Sales Mgr.**